





GENERAL NOTES

1. THIS PLAN IS BASED UPON ON-THE-GROUND INSTRUMENT SURVEYS PERFORMED BY GCG ASSOCIATES IN MARCH 2018 AND SEPTEMBER 2021 AND SUPPLEMENTED WITH ADDITIONAL SURVEY INFORMATION FROM THE TOWN OF LEXINGTON NOVEMBER 2021 THROUGH MAY 2022.

2. LOCATIONS AND DEPTHS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY, AND ARE NOT WARRANTED TO BE CORRECT. UNDERGROUND UTILITIES ARE SHOWN BASED ON AVAILABLE TOWN AND UTILITY RECORDS. ADDITIONAL UTILITIES MAY EXIST WHICH ARE NOT INDICATED ON THESE PLANS. ALL EXISTING UTILITIES SHALL BE VERIFIED FOR SERVICE, SIZE, INVERT ELEVATION, LOCATION, ETC. BY THE CONTRACTOR PRIOR TO NEW CONNECTIONS TO OR OF SAME. CONTRACTOR MUST NOTIFY DIG-SAFE AT 1-888-344-7233 AT LEAST 72 HOURS PRIOR TO ANY CONSTRUCTION. NOTIFY THIS FIRM OF ANY AND ALL DISCREPANCIES PRIOR TO COMMENCING ANY WORK. NAMES AND CONTACTS FOR UTILITY OPERATING AUTHORITIES ARE AVAILABLE UPON REQUEST.

3. HORIZONTAL AND VERTICAL DATUMS REFERENCED FROM NAD OF '83 AND NAVD OF '88.

4. ANY SPOT ELEVATIONS ARE PUBLISHED FOR DESIGN PURPOSES ONLY.

5. THE CONTRACTOR SHALL MAKE DETERMINATIONS OF SUBSURFACE CONDITIONS, INCLUDING THE LOCATION OF ROCK AND THE ACTUAL LOCATION OF UTILITIES OR OTHER FEATURES WHICH MAY AFFECT THE WORK.

6. THE CONTRACTOR SHALL CAUSE AS LITTLE INTERFERENCE AND INCONVENIENCE TO ABUTTERS AS POSSIBLE. THE CONTRACTOR SHALL MAINTAIN SAFE AND CONVENIENT ACCESS TO PRIVATE PROPERTIES AT ALL TIMES.

7. THE FLOW OF TRAFFIC THROUGH CONSTRUCTION ZONES IN EXISTING PUBLIC WAYS SHALL BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE SPECIFIED ON THE PLANS AND IN THE SPECIAL PROVISIONS.

8. A MINIMUM OF 4" LOAM AND SEED SHALL BE PLACED AS NOTED ON THE PLANS AND ON DISTURBED AREAS.

9. PROPERTY LINES SHOWN HEREON ARE COMPILED FROM VARIOUS SOURCES AND DO NOT REPRESENT A PROPERTY LINE RETRACEMENT SURVEY.

10. A MINIMUM UNOBSTRUCTED WIDTH OF 52" SHALL BE MAINTAINED ON ALL PROPOSED SIDEWALKS AND DRIVEWAYS TO ACCOMMODATE THE TOWN OF LEXINGTON'S SIDEWALK SNOWPLOW.

11. MAILBOXES TO BE RESET SHALL BE RESET PER MASSDOT STANDARD DETAIL E 504.1.0 AND AS SHOWN ON THESE PLANS AND DIRECTED BY THE ENGINEER.

12. RESOURCE AREA BOUNDARIES WERE EVALUATED AND FLAGGED BY CHA CONSULTING, INC. ON DECEMBER 28, 2021.

13. THE CONTRACTOR SHALL PROVIDE THE OWNER WITH A CONSTRUCTION SCHEDULE DELINEATING THE SEQUENCE OF WORK, TRAFFIC MANAGEMENT PLAN, AND ESTIMATED TIME OF COMPLETION OF EACH SEGMENT OF WORK, PRIOR TO THE COMMENCEMENT OF WORK.

14. THE CONTRACTOR SHALL MAINTAIN CONTINUOUS TRAFFIC FLOW DURING CONSTRUCTION SATISFACTORY TO THE ENGINEER AND THE TOWN. ACCESS TO ALL EXISTING RESIDENCES SHALL BE MAINTAINED AT ALL TIMES DURING THE COURSE OF CONSTRUCTION BY THE CONTRACTOR. THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE PARKING LOT DURING CONSTRUCTION FOR ALL RESIDENTS. CONTRACTOR SHALL PROVIDE PROPER NOTICE TO ALL RESIDENTS WHEN ACCESS AND EGRESS IS IMPEDED OR OBSTRUCTED. CONTRACTOR CONSTRUCTION SCHEDULE SHALL INCLUDE PROJECT PHASING TO ENSURE MAINTENANCE OF ACCESS AND SUFFICIENT PARKING THROUGHOUT THE CONSTRUCTION PERIOD.

15. NO EQUIPMENT SHALL BE ALLOWED TO BE PARKED ON THE ROAD WHEN NOT IN USE. MATERIALS SHALL NOT BE STOCKPILED ON THE ROAD OR IN PARKING AREAS. THE CONTRACTOR SHALL CONSULT THE LEXINGTON DEPARTMENT OF PUBLIC WORKS WITH RESPECT TO LOCATION OF STOCKPILED MATERIALS.

16. SIDEWALKS, WALKS, AND DRIVEWAYS THAT ARE DAMAGED OR REMOVED DURING CONSTRUCTION SHALL BE REPLACED WITH THE SAME TYPE OF MATERIAL ONCE THE WORK IS COMPLETED.

17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING ANY DEBRIS, SEDIMENT, OR SILTY WATER FROM ENTERING DRAINAGE SYSTEM, ETC., DURING ALL PHASES OF CONSTRUCTION. CONTROLS MAY INCLUDE HAY BALES, SILT FENCE, SILT SACKS, CRUSHED STONE.
18. ALL CONSTRUCTION MATERIAL, DEBRIS, ASPHALT, SOIL, ETC. THAT IS REMOVED FROM THE SITE SHALL BE HANDLED AND DISPOSED OF IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS.
19. DURING CONSTRUCTION, THE CONTRACTOR SHALL PROTECT ALL PLANTS, TREES AND ROOTS OF TREES TO REMAIN. THE CONTRACTOR SHALL CONSULT WITH A CERTIFIED ARBORIST AND IMPLEMENT THE RECOMMENDED TREE & PLANT PROTECTION MEASURES. THIS MAY INCLUDE TREE & PLANT PROTECTION FENCE, TREE ARMORING, ROOT PRUNING, AND AIR EXCAVATION. ADDITIONAL TREE REMOVALS MAY BE REQUIRED IN ADDITION TO WHAT IS SHOWN ON THESE PLANS AS DIRECTED BY THE ENGINEER.
20. THE CONTRACTOR SHALL MAINTAIN THE EXISTING SITE DRAINAGE PATTERNS UNLESS OTHERWISE NOTED. ALL GRADING MODIFICATIONS SHALL DIRECT DRAINAGE AWAY FROM EXISTING BUILDINGS AND TOWARDS THE APPROPRIATE AREAS. ALL GRADING MODIFICATIONS SHALL BE GRADUAL SO AS NOT TO CREATE ANY STEEP SLOPES, UNEVEN AREAS, ETC.
21. DURING THE COURSE OF CONSTRUCTION, ANY DAMAGE TO FENCES, PATHS, STAIRS, AND VEGETATION SHALL BE REPAIRED OR REPLACED AND RESTORED TO THE ORIGINAL CONDITION AT NO ADDITIONAL EXPENSE TO THE OWNER.
22. ALL CASTINGS, GATE BOXES, HYDRANTS, LIGHT POLES, ETC. DAMAGED DURING CONSTRUCTION SHALL BE SUPPLIED AND REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
23. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR CONSTRUCTION MEANS, METHODS, TECHNIQUES, AND PROCEDURES, AND FOR SAFETY PRECAUTIONS AND PROGRAMS IN CONNECTION WITH ALL WORK INCLUDED UNDER THIS CONTRACT. THE DRAWINGS DO NOT INCLUDE NECESSARY COMPONENTS FOR CONSTRUCTION SAFETY. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR PROVIDING AND MAINTAINING ALL SAFETY BARRIERS, WARNING FLASHERS AND THE LIKE, AS REQUIRED BY THE CONDUCT OF THE WORK FOR THE PROTECTION OF WORKERS AND NON-WORKERS ALIKE. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO OSHA REQUIREMENTS.
24. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SITE RESTORATION AND CLEAN-UP UPON COMPLETION OF THE PROJECT.
25. THE FINAL LOCATION OF PROPOSED TREES SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.

GENERAL SYMBOLS

EXISTING	PROPOSED	DESCRIPTION
		BALANCED STONE WALL
		BORDER OF WETLAND
		BUILDING
		BUSH / HEDGES
		CABLE MANHOLE
		CATCH BASIN
		COUNTY BOUND
		CURBING
		DRAIN LINE
		DRAINAGE MANHOLE
		ELECTRIC MANHOLE
		ELECTRIC HANDHOLE
		FENCE GATE POST
		GAS LINE
		GAS MANHOLE
		GAS PUMP
		GAS VALVE
		GPS POINT
		GUTTER INLET
		HYDRANT
		LIGHT POLE
		LIMIT OF EDGE OF PAVEMENT
		MAG NAIL
		MAIL BOX
		MAJOR CONTOURS
		MINOR CONTOURS
		MISC MANHOLE
		OVERHEAD CABLE/WIRE
		POST CIRCULAR
		POST SQUARE
		PROPERTY LINE OR APPROXIMATE PROPERTY LINE
		RETAINING WALL
		SAWCUT LINE
		SEDIMENT CONTROL BARRIER
		SEWER MANHOLE
		SEWER LINE
		SIGN
		STONE BOUND
		STUMP
		TELEPHONE MANHOLE
		TOP OR BOTTOM OF SLOPE
		TRAVERSE POINT
		TREE LINE
		TREE W/ DIAMETER
		TROLLEY POLE OR GUY POLE
		UTILITY POLE
		WATER MANHOLE
		WATER MAIN
		WATER SERVICE
		WATER SHUT OFF
		WATER VALVE
		WETLAND BUFFER
		WOOD FENCE

PAVEMENT MARKINGS SYMBOLS

EXISTING	PROPOSED	DESCRIPTION
		STOP LINE - 12" WIDE
		CROSSWALK - SEE SHEET 13 FOR DETAIL
		SOLID WHITE LINE
		DOUBLE YELLOW LINE - 6" WIDE LINES WITH 6" WIDE GAP

ABBREVIATIONS		ABBREVIATIONS (CONT.)	
GENERAL		GENERAL	
AADT	ANNUAL AVERAGE DAILY TRAFFIC	CONST	CONSTRUCTION
ABAN	ABANDON	CR GR	CROWN GRADE
ADJ	ADJUST	DHV	DESIGN HOURLY VOLUME
APPROX.	APPROXIMATE	DI	DROP INLET
A.C.	ASPHALT CONCRETE	DIA	DIAMETER
ACCM PIPE	ASPHALT COATED CORRUGATED METAL PIPE	DIP	DUCTILE IRON PIPE
BBC	BITUMINOUS BERM CURB	DW	STEADY DON'T WALK - PORTLAND ORANGE
BIT.	BITUMINOUS	DWY	DRIVEWAY
BC	BOTTOM OF CURB	ELEV (or EL.)	ELEVATION
BCC	BOTTOM OF CURVED CURB	EMB	EMBANKMENT
BD.	BOUND	EOP	EDGE OF PAVEMENT
BL	BASELINE	EXIST (or EX)	EXISTING
BLDG	BUILDING	EXC	EXCAVATION
BM	BENCHMARK	F&C	FRAME AND COVER
BO	BY OTHERS	F&G	FRAME AND GRATE
BOS	BOTTOM OF SLOPE	FDN.	FOUNDATION
BR.	BRIDGE	FLDSTN	FIELDSTONE
CB	CATCH BASIN	GAR	GARAGE
CBCI	CATCH BASIN WITH CURB INLET	GD	GROUND
CC	CEMENT CONCRETE	GG	GAS GATE
CCB	CAPE COD BERM	GI	GUTTER INLET
CCM	CEMENT CONCRETE MASONRY	GIP	GALVANIZED IRON PIPE
CEM	CEMENT	GRAN	GRANITE
CI	CURB INLET	GRAV	GRAVEL
CIP	CAST IRON PIPE	GRD	GUARD
CLF	CHAIN LINK FENCE	HDW	HEADWALL
CL	CENTERLINE	HMA	HOT MIX ASPHALT
CMP	CORRUGATED METAL PIPE	HOR	HORIZONTAL
CSP	CORRUGATED STEEL PIPE	HYD	HYDRANT
CO.	COUNTY	INV	INVERT
CONC	CONCRETE	JCT	JUNCTION
CONT	CONTINUOUS	L	LENGTH OF CURVE

ABBREVIATIONS (CONT.)	
GENERAL	
LB	LEACH BASIN
LP	LIGHT POLE
LT	LEFT
MAX	MAXIMUM
MB	MAILBOX
MH	MANHOLE
MHB	MASSACHUSETTS HIGHWAY BOUND
MIN	MINIMUM
NIC	NOT IN CONTRACT
NO.	NUMBER
PC	POINT OF CURVATURE
PCC	POINT OF COMPOUND CURVATURE
PCR	PEDESTRIAN CURB RAMP
P.G.L.	PROFILE GRADE LINE
PI	POINT OF INTERSECTION
POC	POINT ON CURVE
POT	POINT ON TANGENT
PRC	POINT OF REVERSE CURVATURE
PROJ	PROJECT
PROP	PROPOSED
PSB	PLANTABLE SOIL BORROW
PT	POINT OF TANGENCY
PVC	POINT OF VERTICAL CURVATURE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENCY
PVMT	PAVEMENT
PWW	PAVED WATER WAY
R	RADIUS OF CURVATURE
R&D	REMOVE AND DISPOSE
RCP	REINFORCED CONCRETE PIPE
RD	ROAD
RDWY	ROADWAY

ABBREVIATIONS (CONT.)	
GENERAL	
REM	REMOVE
RET	RETAIN
RET WALL	RETAINING WALL
ROW	RIGHT OF WAY
RR	RAILROAD
R&R	REMOVE AND RESET
R&S	REMOVE AND STACK
RT	RIGHT
SB	STONE BOUND
SHLD	SHOULDER
SMH	SEWER MANHOLE
ST	STREET
STA	STATION
SSD	STOPPING SIGHT DISTANCE
SHLO	STATE HIGHWAY LAYOUT LINE
SW	SIDEWALK
T	TANGENT DISTANCE OF CURVE/TRUCK %
TAN	TANGENT
TEMP	TEMPORARY
TC	TOP OF CURB
TOS	TOP OF SLOPE
TYP	TYPICAL
UP	UTILITY POLE
VAR	VARIES
VERT	VERTICAL
VC	VERTICAL CURVE
WG	WATER GATE
WIP	WROUGHT IRON PIPE
WM	WATER METER/WATER MAIN
X-SECT	CROSS SECTION

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TOWN OF LEXINGTON, MASSACHUSETTS

HIGHWAY DEPARTMENT

LINCOLN STREET SIDEWALK PROJECT



6/14/22

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

LEGEND & ABBREVIATIONS

No.	Submittal / Revision	App'd.	By	Date
1	CONCEPT			10/22/21
2	CONCEPT - REV 1			01/05/22
3	PRELIM			03/02/22
4	PRELIM - REV 1			03/09/22
5	FINAL			06/02/22
6	FINAL REV 1			06/14/22

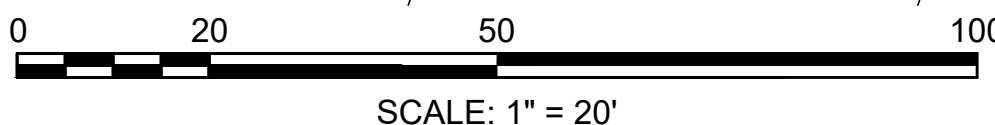
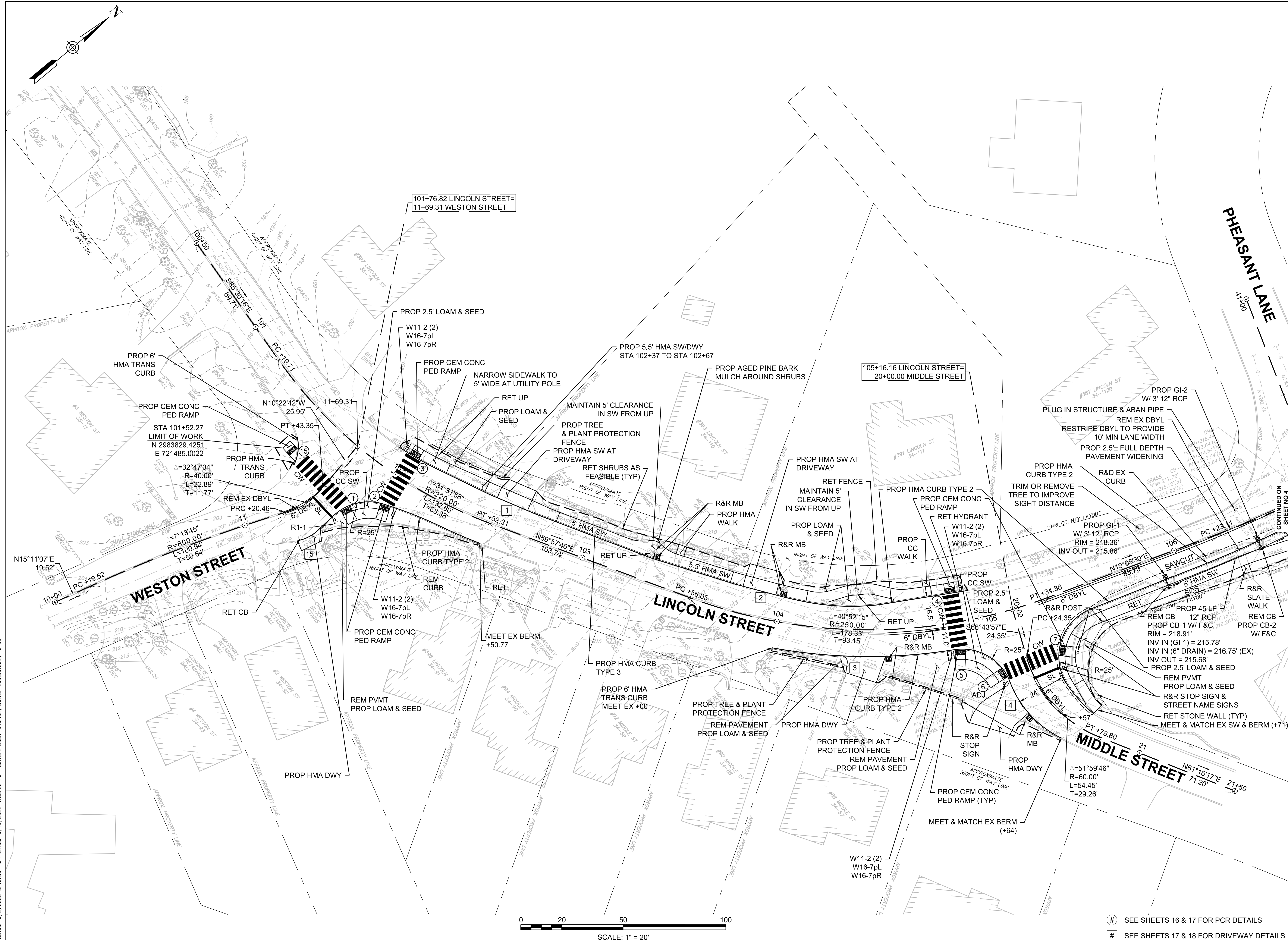
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Issue Date: 06-14-2022	Project No: 71414	Scale: NTS

Drawing No.:

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- # SEE SHEETS 16 & 17 FOR PCR DETAILS
- # SEE SHEETS 17 & 18 FOR DRIVEWAY DETAILS

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TOWN OF  
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HIGHWAY  
DEPARTMENT

LINCOLN STREET  
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CONSTRUCTION  
PLANS - 1

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Designed By: DC/JB

Drawn By: DC/JB

Checked By: JGM

Issue Date: 06-14-2022

Project No: 71414

Scale: 1" = 20'

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**3** OF **20**

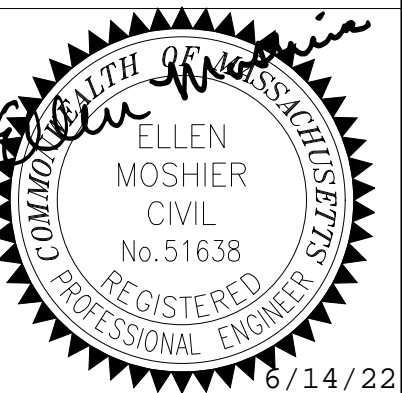




TOWN OF  
LEXINGTON,  
MASSACHUSETTS

HIGHWAY  
DEPARTMENT

LINCOLN STREET  
SIDEWALK  
PROJECT



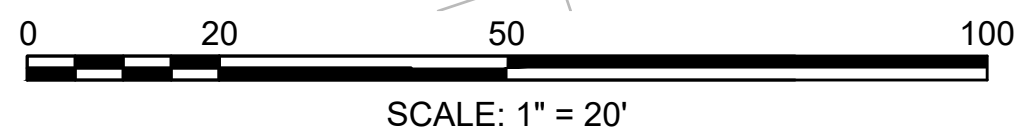
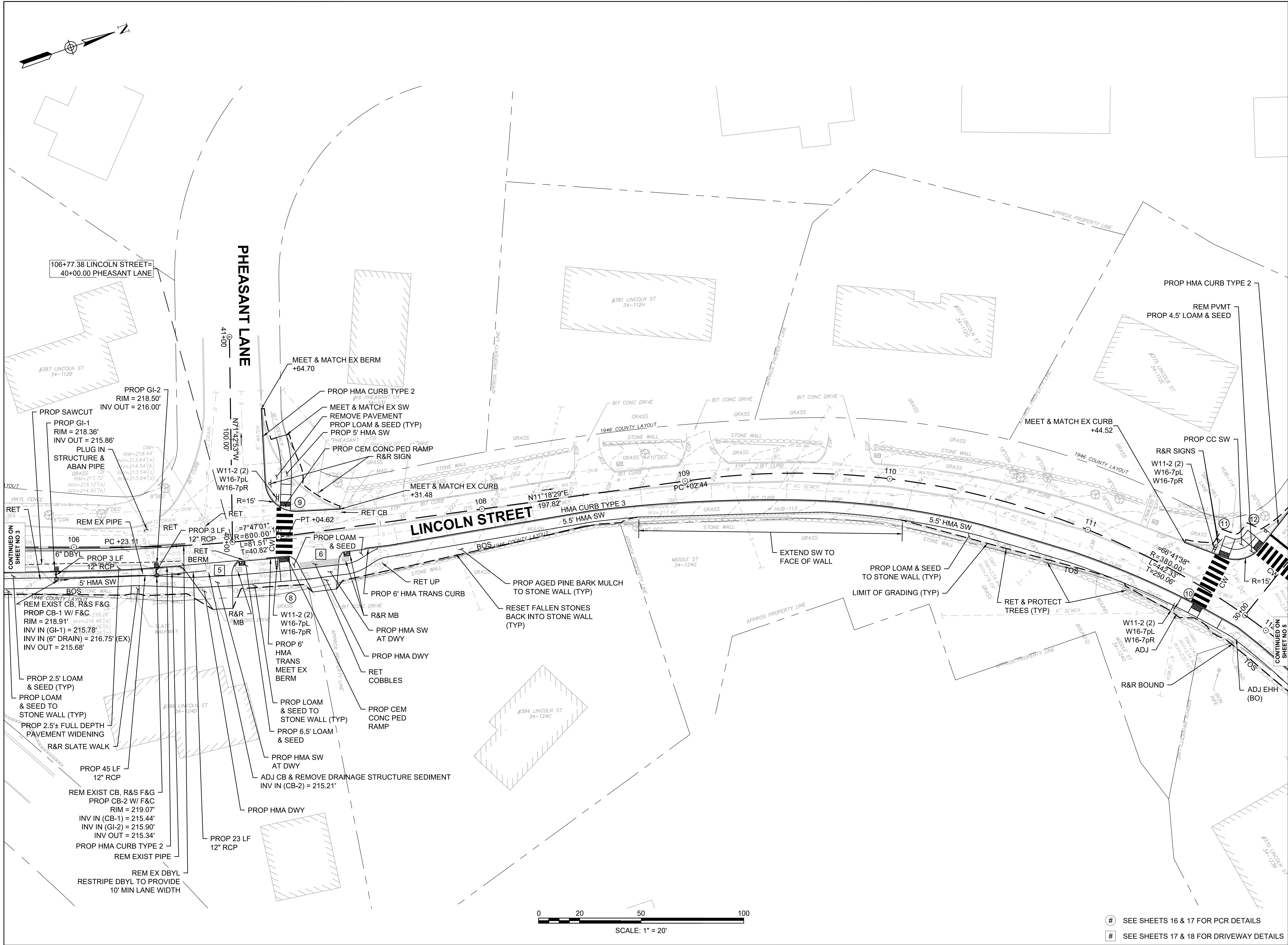
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## CONSTRUCTION PLANS - 2

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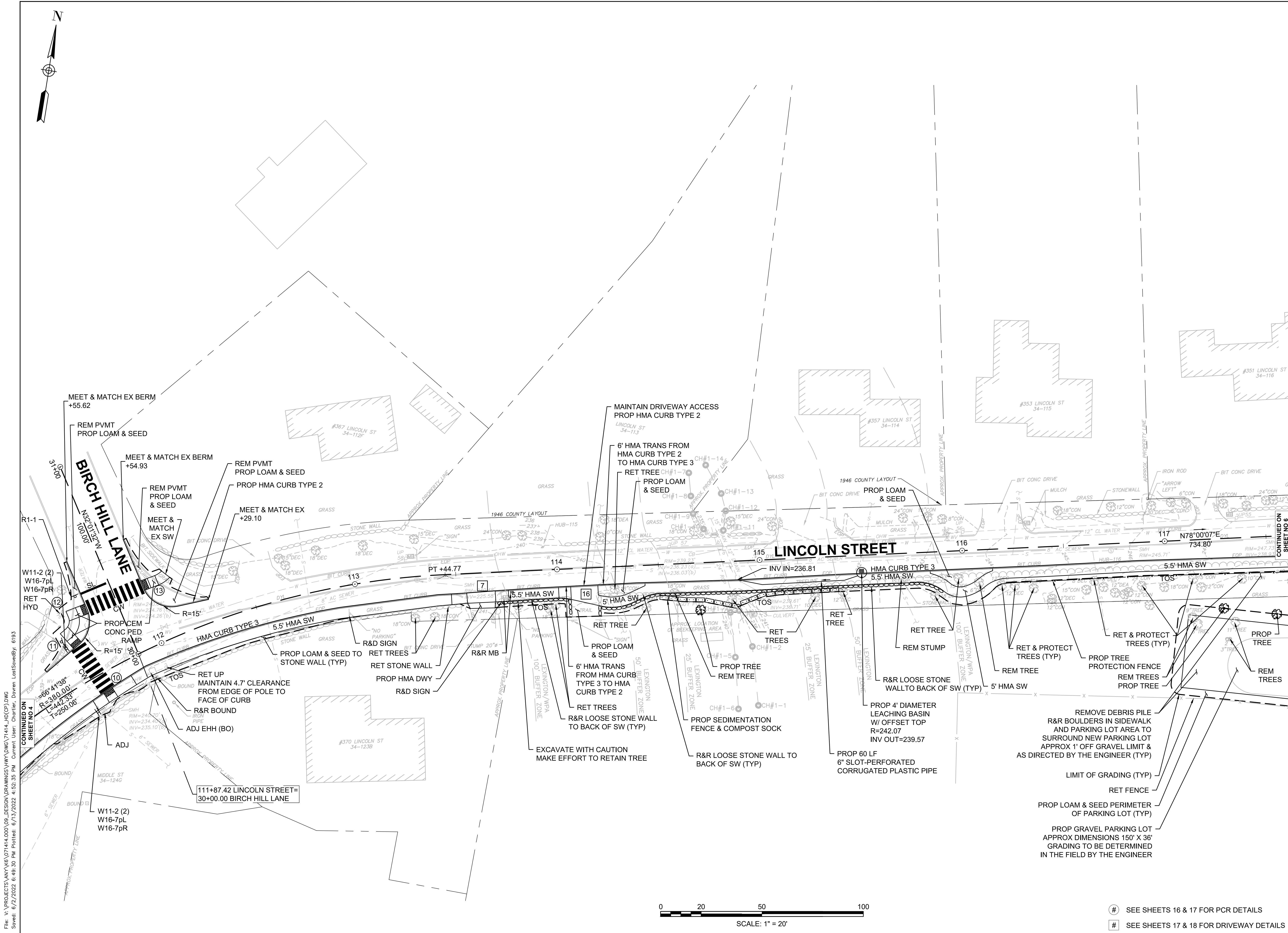
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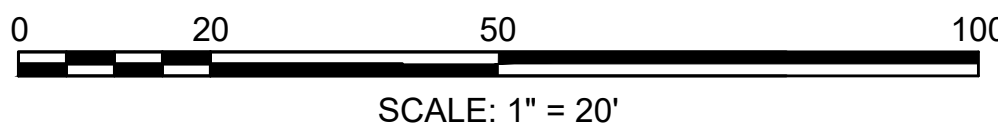


- # SEE SHEETS 16 & 17 FOR PCR DETAILS
- # SEE SHEETS 17 & 18 FOR DRIVEWAY DETAILS





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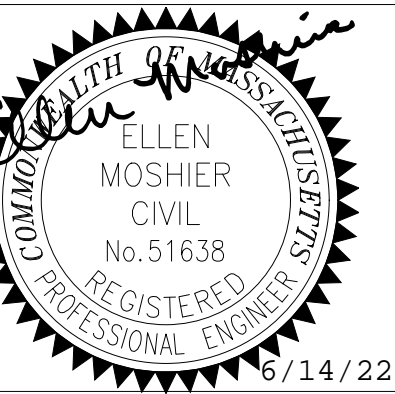
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LINCOLN STREET  
SIDEWALK  
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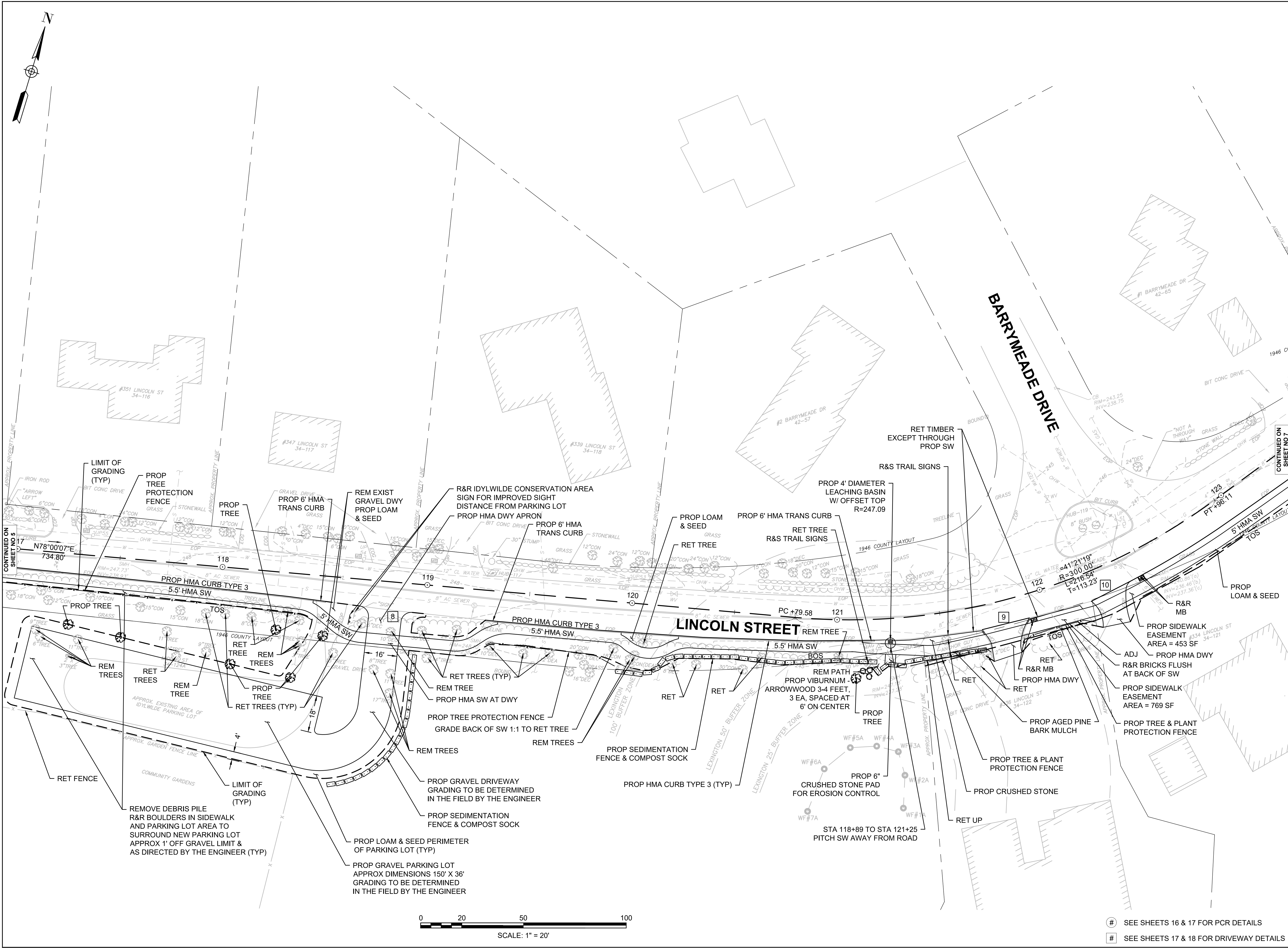
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CONSTRUCTION  
PLANS - 3

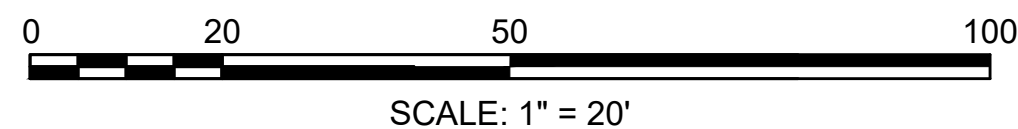
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TOWN OF  
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SIDEWALK  
PROJECT



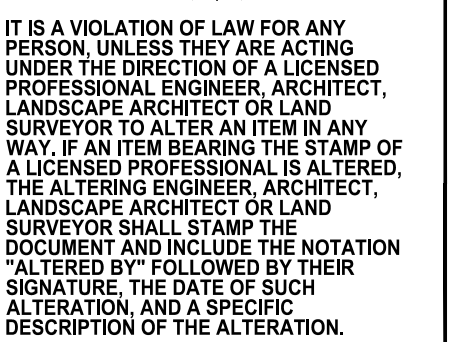
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### CONSTRUCTION PLANS - 4

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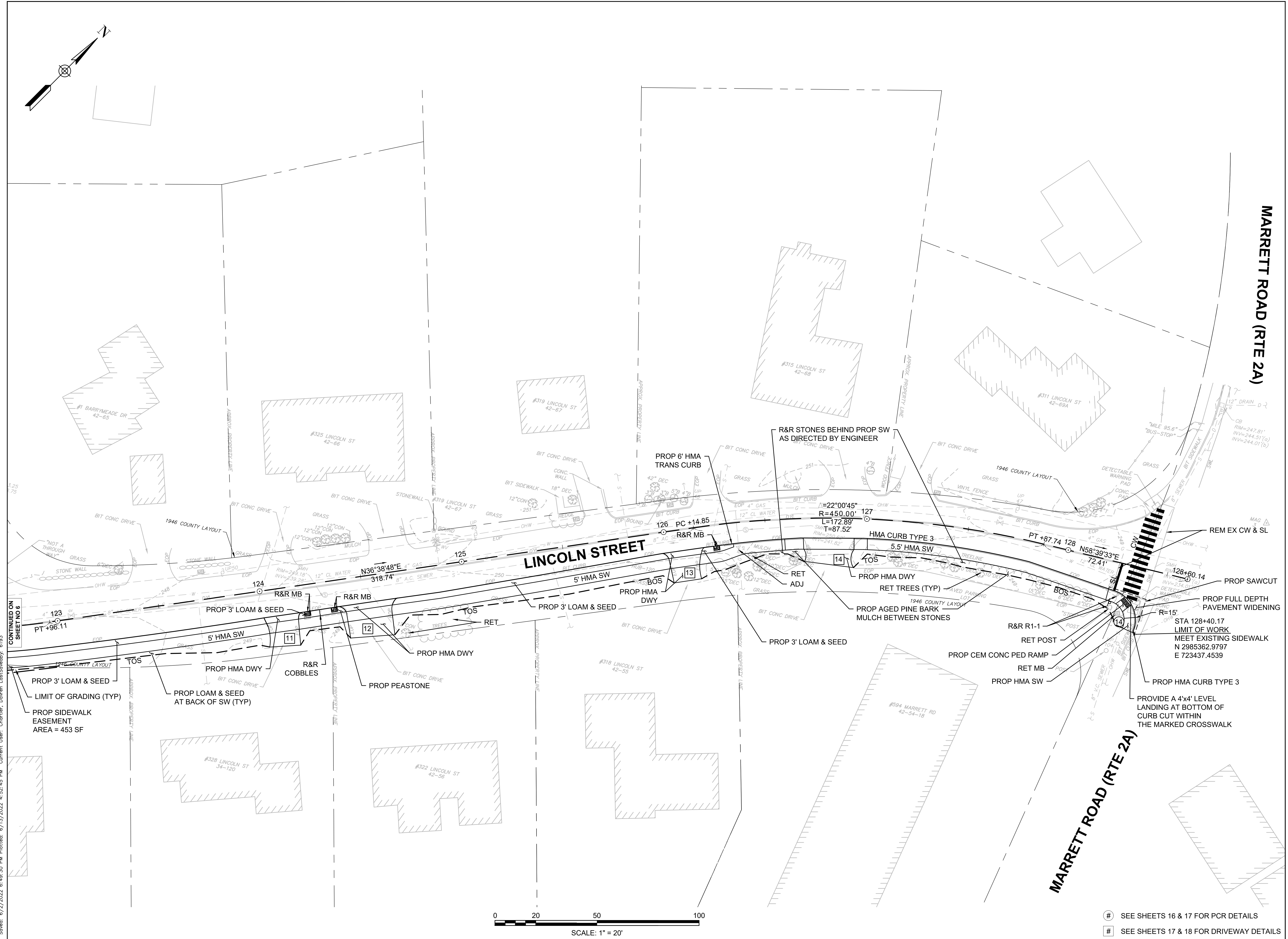


LINCOLN STREET  
SIDEWALK  
PROJECT

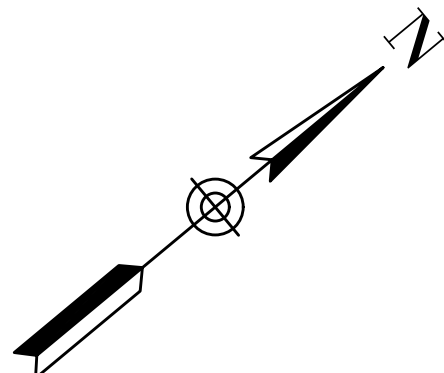
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FINAL REV 1			06/14/22

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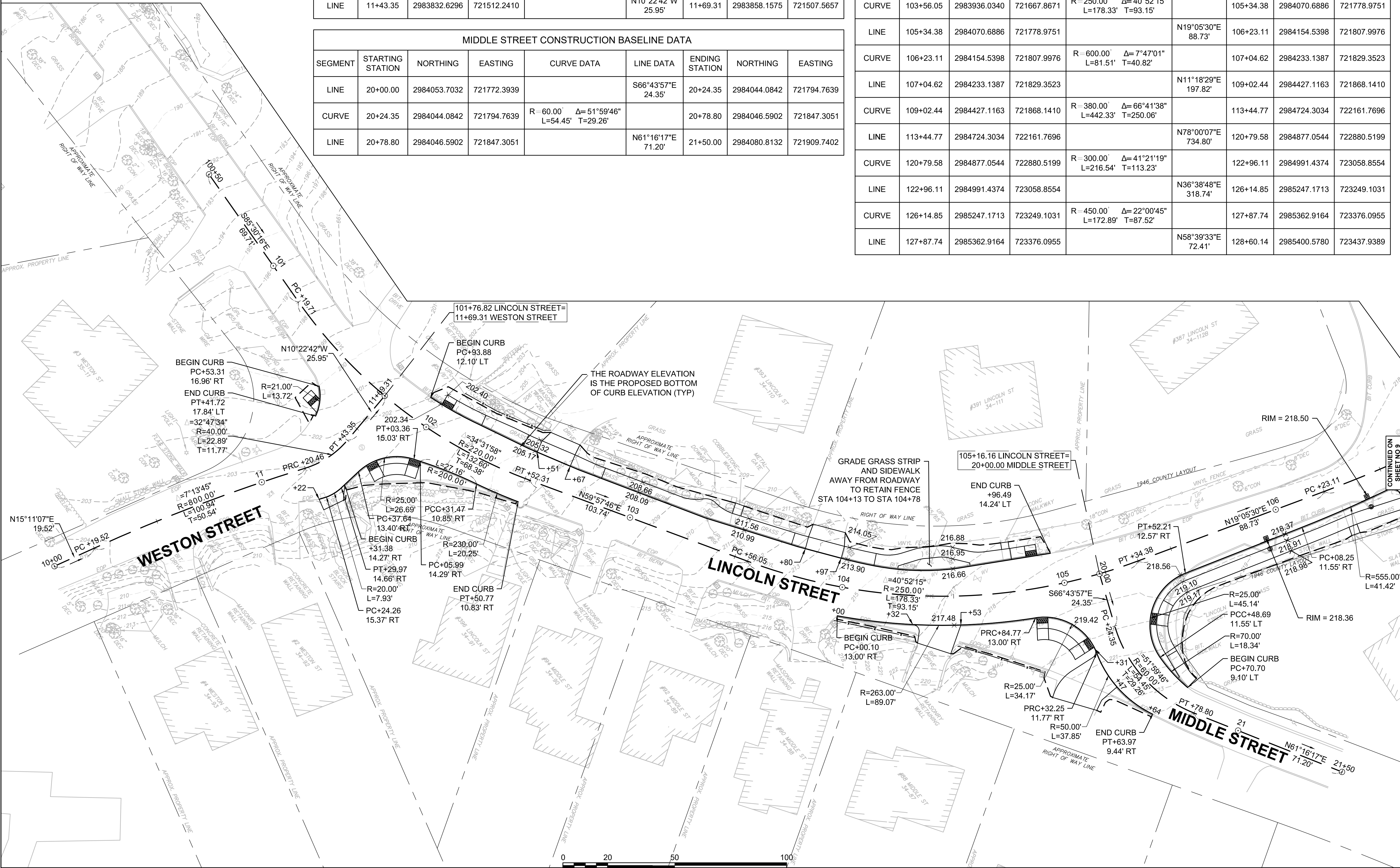




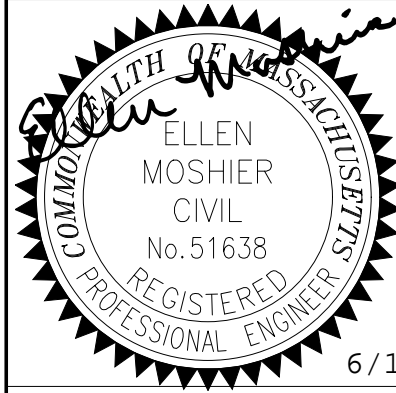
WESTON STREET CONSTRUCTION BASELINE DATA								
SEGMENT	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
LINE	10+00.00	2983695.8413	721472.2525		N15°11'07"E 19.52'	10+19.52	2983714.6801	721477.3657
CURVE	10+19.52	2983714.6801	721477.3657	R=800.00' Δ=7°13'45" L=100.94' T=50.54'		11+20.46	2983810.1716	721509.8734
CURVE	11+20.46	2983810.1716	721509.8734	R=40.00' Δ=32°47'34" L=22.89' T=11.77'		11+43.35	2983832.6296	721512.2410
LINE	11+43.35	2983832.6296	721512.2410		N10°22'42"W 25.95'	11+69.31	2983858.1575	721507.5657

MIDDLE STREET CONSTRUCTION BASELINE DATA								
SEGMENT	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
LINE	20+00.00	2984053.7032	721772.3939		S66°43'57"E 24.35'	20+24.35	2984044.0842	721794.7639
CURVE	20+24.35	2984044.0842	721794.7639	R=60.00' Δ=51°59'46" L=54.45' T=29.26'		20+78.80	2984046.5902	721847.3051
LINE	20+78.80	2984046.5902	721847.3051		N61°16'17"E 71.20'	21+50.00	2984080.8132	721909.7402

LINCOLN STREET CONSTRUCTION BASELINE DATA								
SEGMENT	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
LINE	100+50.00	2983860.6990	721381.1943		S85°30'16"E 69.71'	101+19.71	2983855.2351	721450.6896
CURVE	101+19.71	2983855.2351	721450.6896	R=220.00' Δ=34°31'58" L=132.60' T=68.38'		102+52.31	2983884.1044	721578.0574
LINE	102+52.31	2983884.1044	721578.0574		N59°57'46"E 103.74'	103+56.05	2983936.0340	721667.8671
CURVE	103+56.05	2983936.0340	721667.8671	R=250.00' Δ=40°52'15" L=178.33' T=93.15'		105+34.38	2984070.6886	721778.9751
LINE	105+34.38	2984070.6886	721778.9751		N19°05'30"E 88.73'	106+23.11	2984154.5398	721807.9976
CURVE	106+23.11	2984154.5398	721807.9976	R=600.00' Δ=7°47'01" L=81.51' T=40.82'		107+04.62	2984233.1387	721829.3523
LINE	107+04.62	2984233.1387	721829.3523		N11°18'29"E 197.82'	109+02.44	2984427.1163	721868.1410
CURVE	109+02.44	2984427.1163	721868.1410	R=380.00' Δ=66°41'38" L=442.33' T=250.06'		113+44.77	2984724.3034	722161.7696
LINE	113+44.77	2984724.3034	722161.7696		N78°00'07"E 734.80'	120+79.58	2984877.0544	722880.5199
CURVE	120+79.58	2984877.0544	722880.5199	R=300.00' Δ=41°21'19" L=216.54' T=113.23'		122+96.11	2984991.4374	723058.8554
LINE	122+96.11	2984991.4374	723058.8554		N36°38'48"E 318.74'	126+14.85	2985247.1713	723249.1031
CURVE	126+14.85	2985247.1713	723249.1031	R=450.00' Δ=22°00'45" L=172.89' T=87.52'		127+87.74	2985362.9164	723376.0955
LINE	127+87.74	2985362.9164	723376.0955		N58°39'33"E 72.41'	128+60.14	2985400.5780	723437.9389



TOWN OF  
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MASSACHUSETTS  
  
HIGHWAY  
DEPARTMENT  
  
LINCOLN STREET  
SIDEWALK  
PROJECT



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CURB TIE &  
GRADING  
PLANS - 1

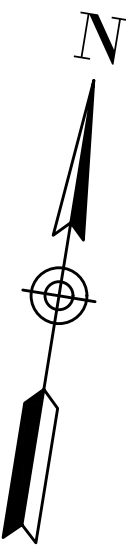
No.	Submittal / Revision	App'd. By	Date
1	CONCEPT		10/22/21
2	CONCEPT - REV 1		01/05/22
3	PRELIM		03/02/22
4	PRELIM - REV 1		03/09/22
5	FINAL		06/02/22
6	FINAL REV 1		06/14/22

Designed By: DC/JB	Drawn By: DC/JB	Checked By: JGM
Issue Date: 06-14-2022	Project No: 71414	Scale: 1" = 20'

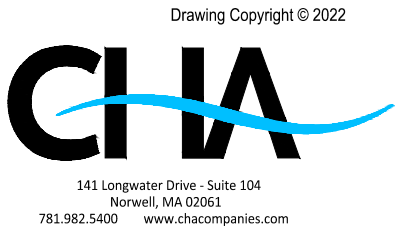
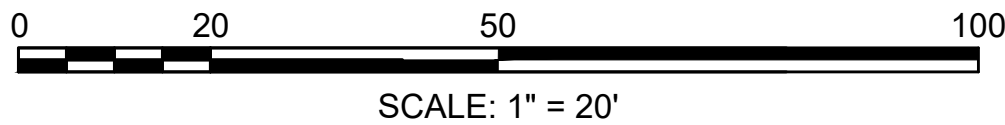
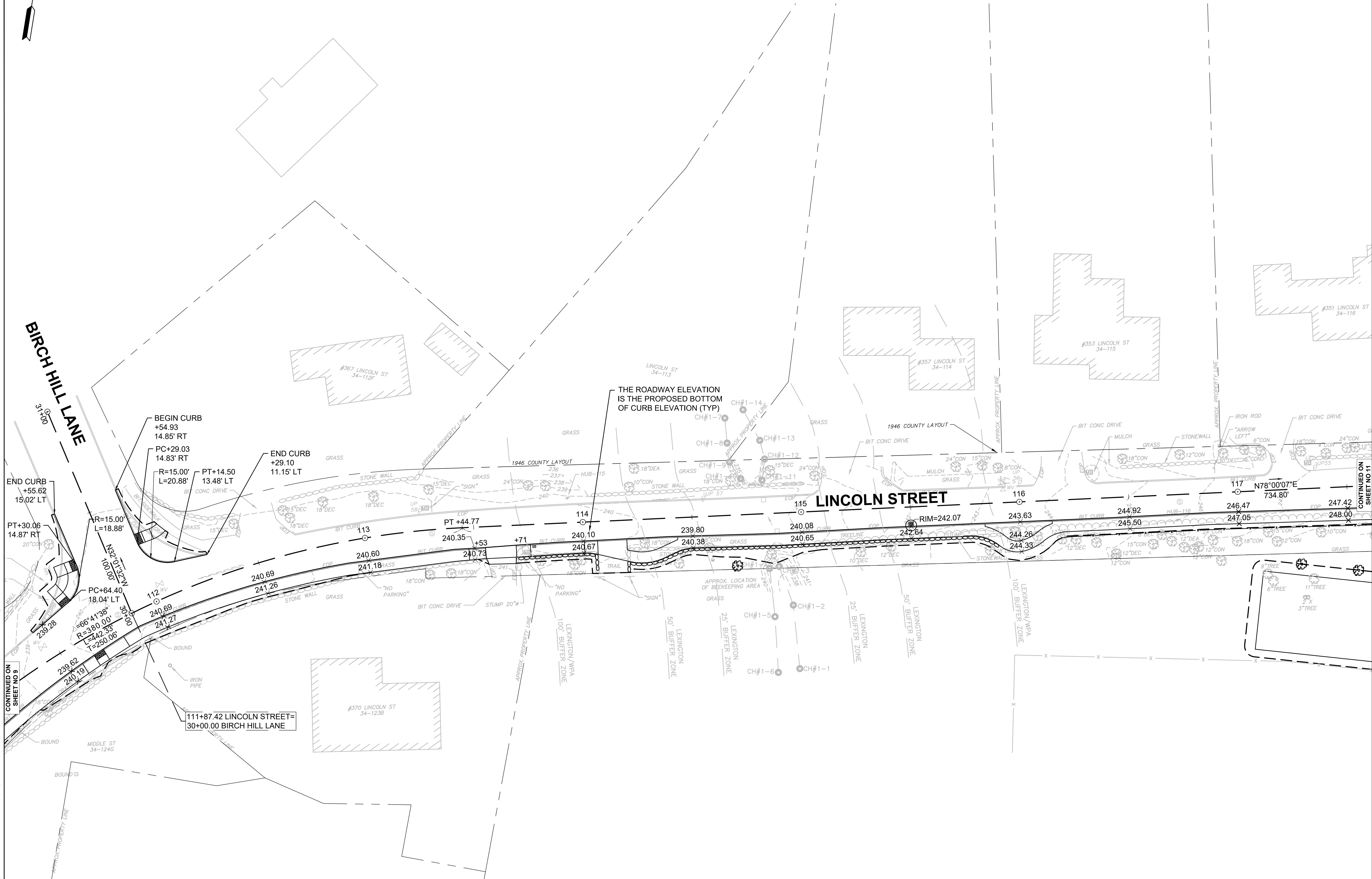








BIRCH HILL LANE CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
LINE	30+00.00	2984661.1066	722018.8933		N32°01'32"W 100.00'	31+00.00	2984745.8837	721965.8662



TOWN OF  
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LINCOLN STREET  
SIDEWALK  
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CURB TIE &  
GRADING  
PLANS - 3

No.	Submittal/ Revision	App'd. By	Date
1	CONCEPT		10/22/21
2	CONCEPT - REV 1		01/05/22
3	PRELIM		03/02/22
4	PRELIM - REV 1		03/09/22
5	FINAL		06/02/22
6	FINAL REV 1		06/14/22

Designed By: DC/JB	Drawn By: DC/JB	Checked By: JGM
Issue Date: 06-14-2022	Project No.: 71414	Scale: 1" = 20'

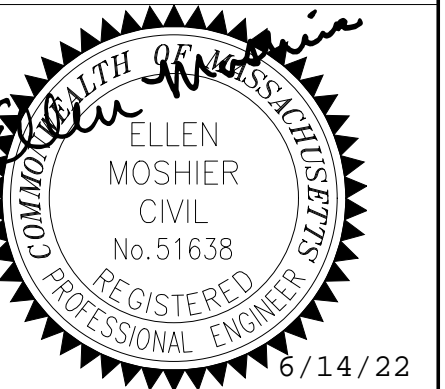
Drawing No.:

10 OF 20





TOWN OF  
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HIGHWAY  
DEPARTMENTLINCOLN STREET  
SIDEWALK  
PROJECT

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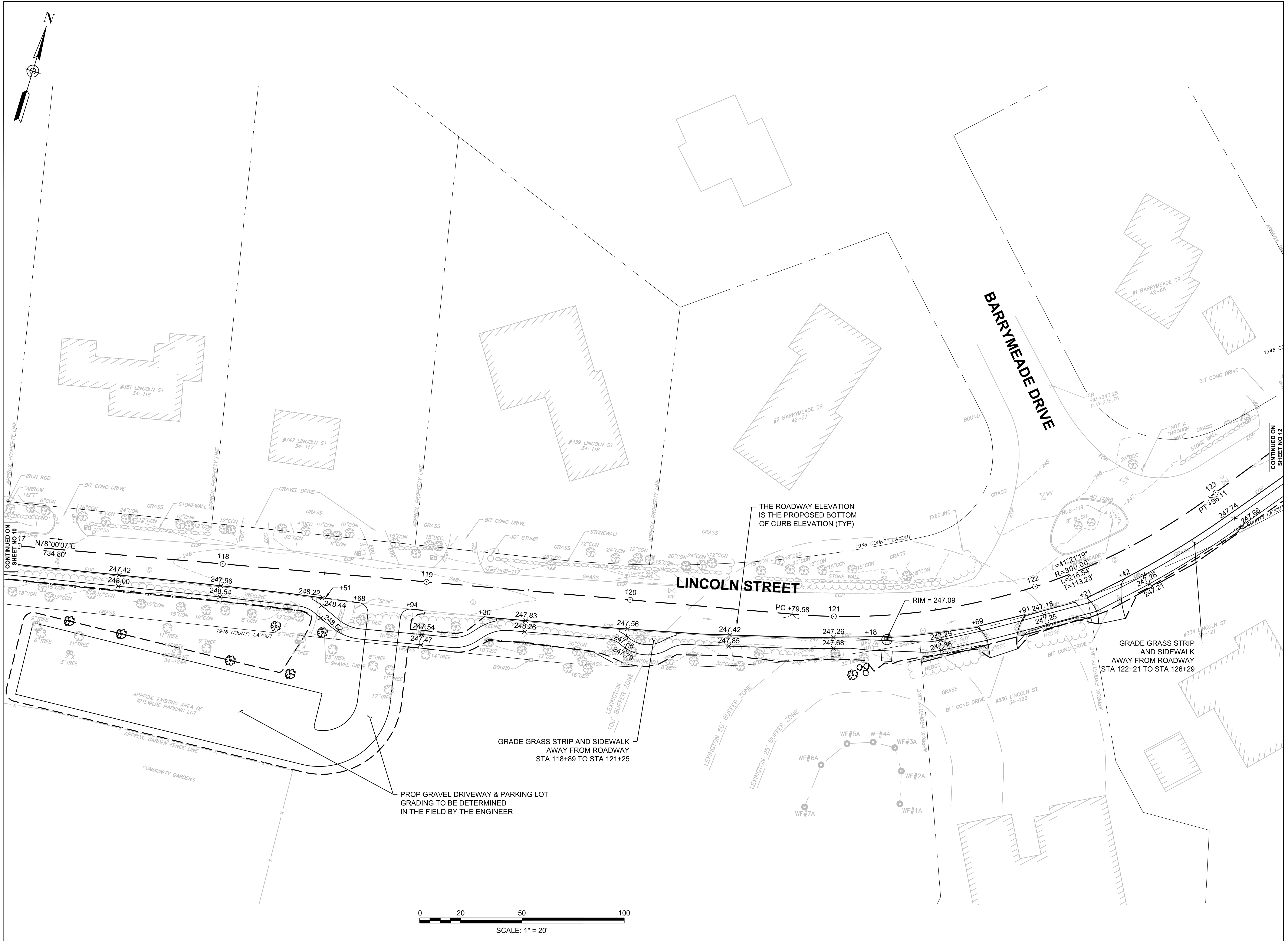
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GRADING  
PLANS - 4**

No.	Submittal / Revision	App'd.	By	Date
1	CONCEPT			10/22/21
2	CONCEPT - REV 1			01/05/22
3	PRELIM			03/02/22
4	PRELIM - REV 1			03/09/22
5	FINAL			06/02/22
6	FINAL REV 1			06/14/22

Designed By: DC/JB	Drawn By: DC/JB	Checked By: JGM
Issue Date: 06-14-2022	Project No: 71414	Scale: 1" = 20'

Drawing No.:

1 OF 20

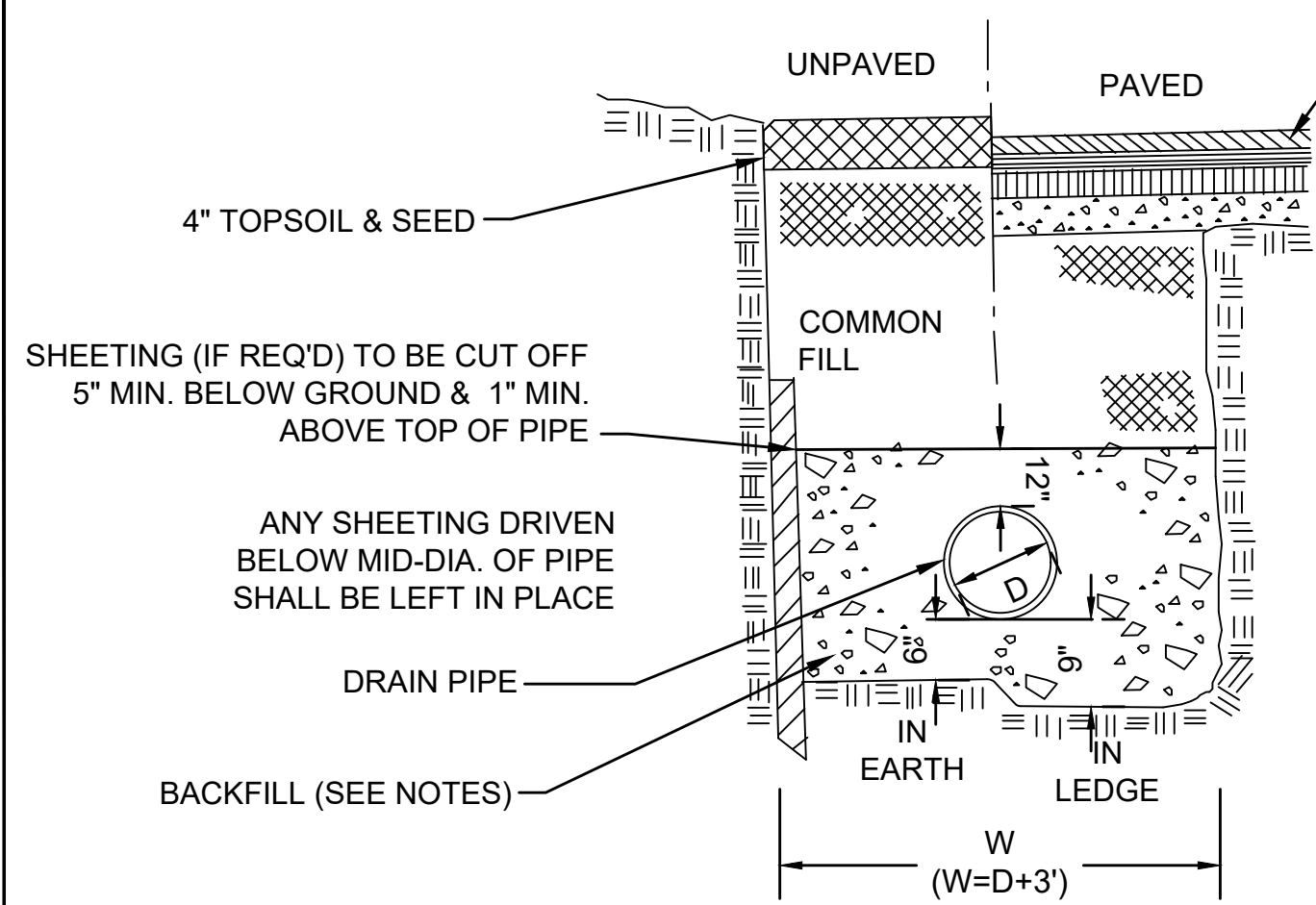




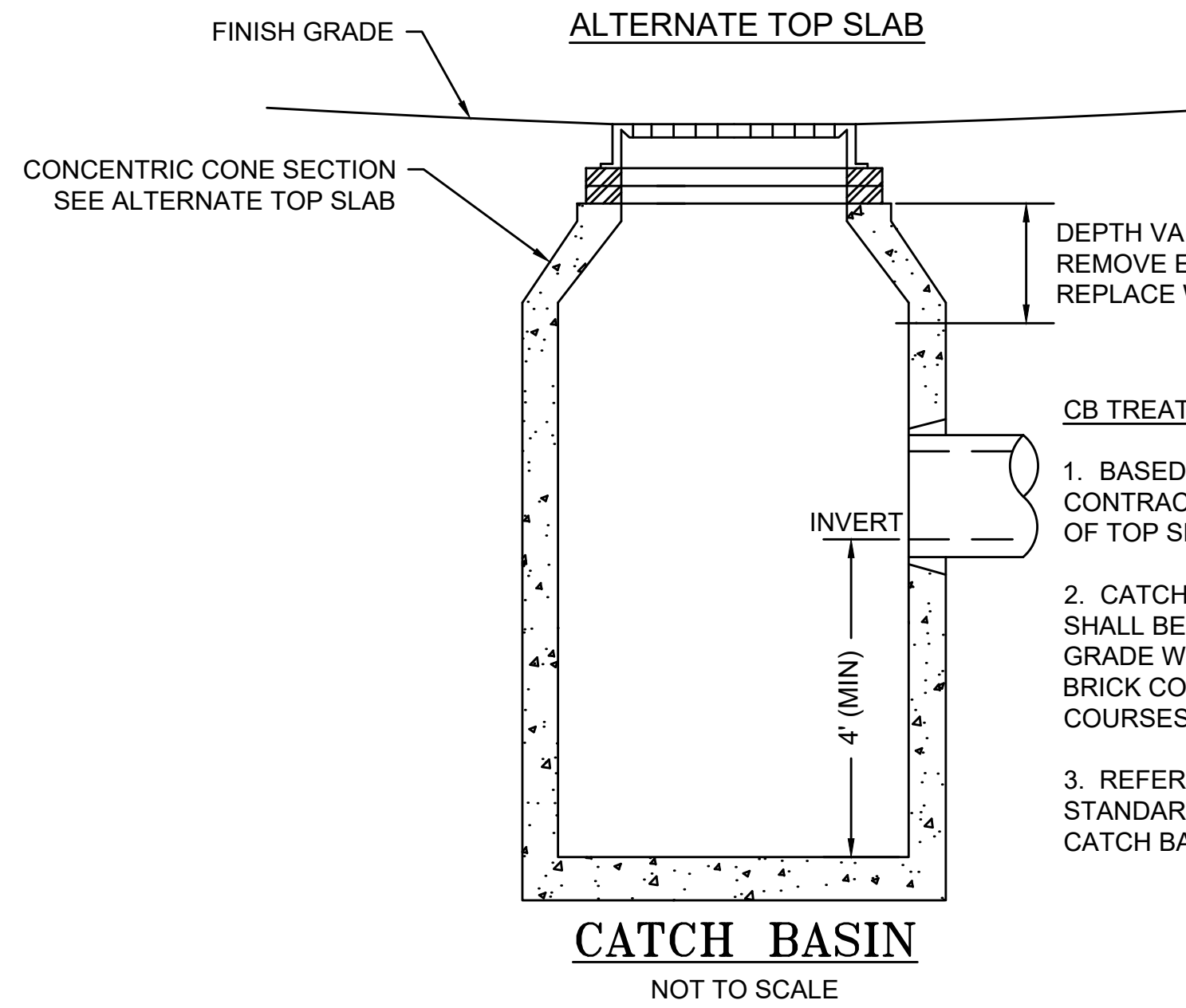
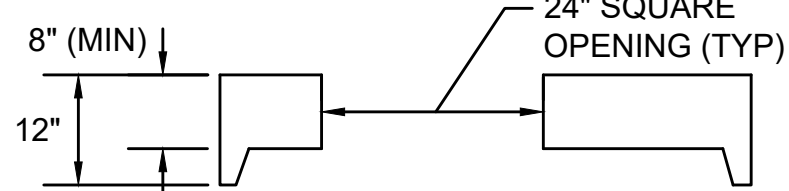
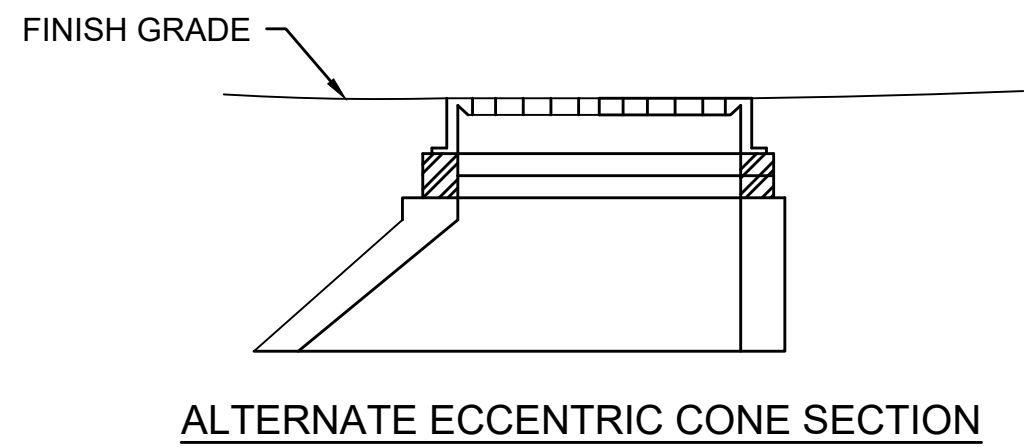
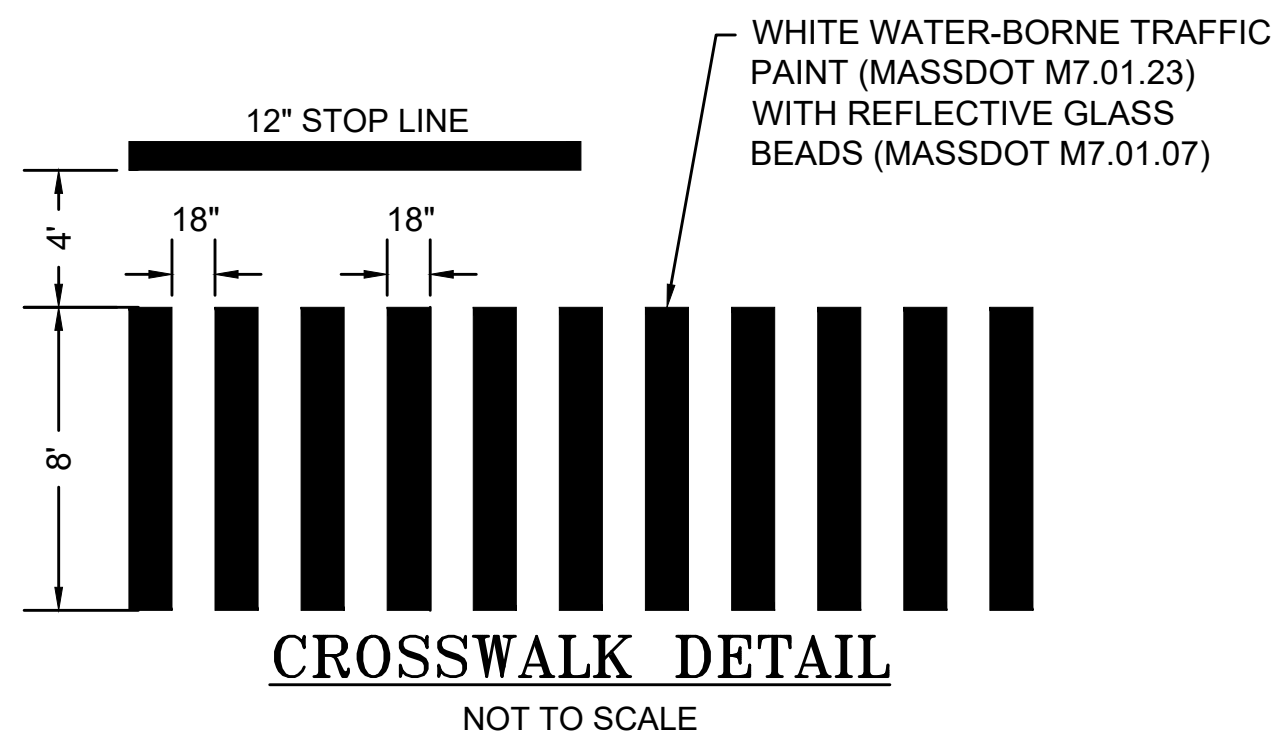




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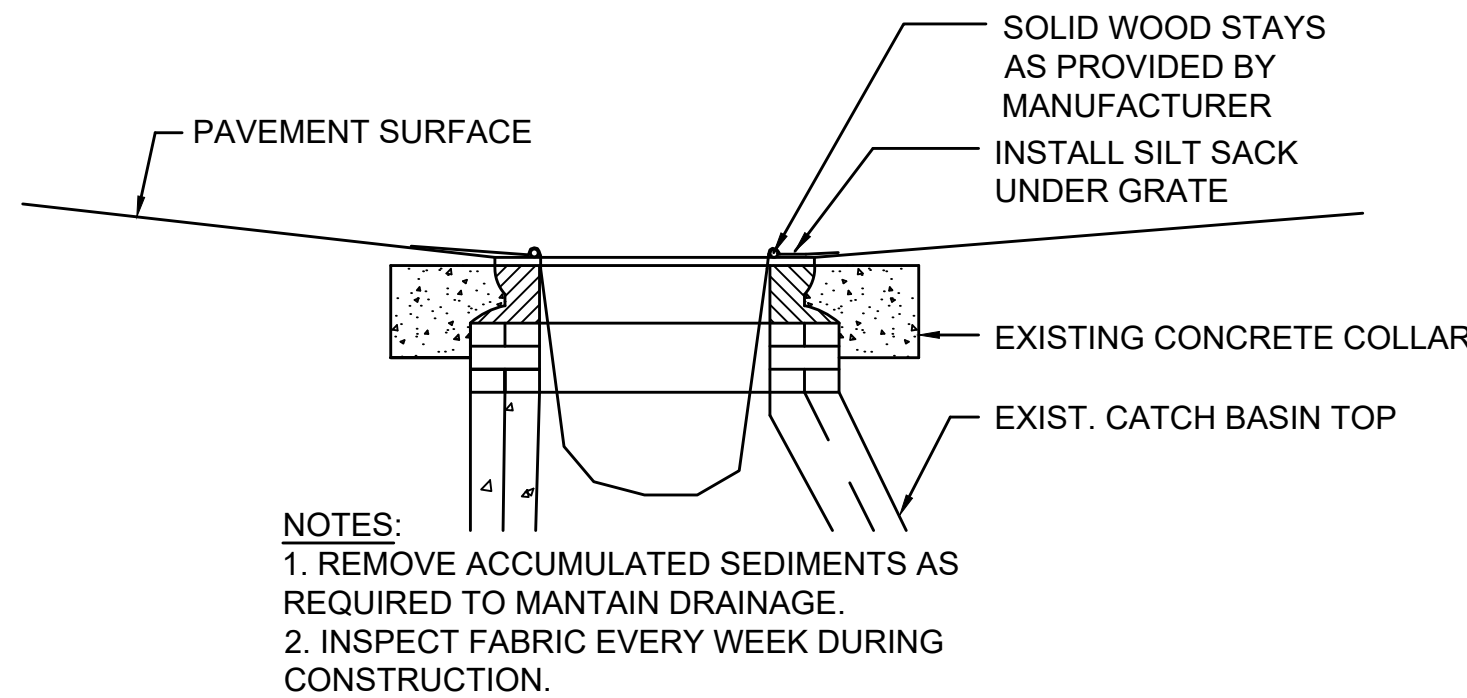
**TYPICAL TRENCH SECTIONS**  
NOT TO SCALE



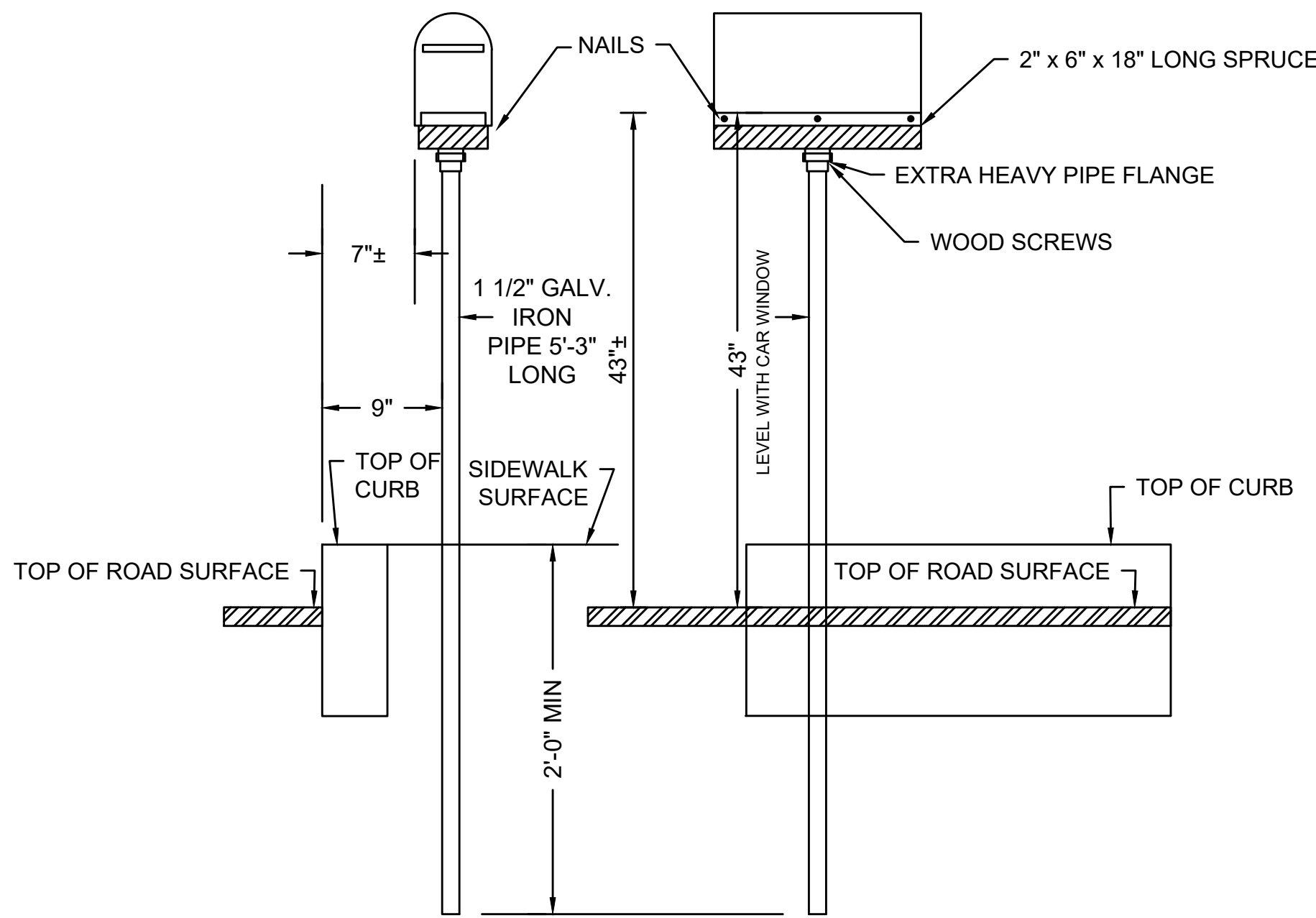
**CB TREATMENT NOTES**

1. BASED ON ACTUAL FIELD CONDITIONS, THE CONTRACTOR SHALL DETERMINE WHICH STYLE OF TOP SECTION SHOULD BE USED.
2. CATCH BASIN FRAME AND GRATE OR COVER SHALL BE SET IN FULL MORTAR BED. ADJUST TO GRADE WITH CLAY BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM).
3. REFER TO MASSDOT CONSTRUCTION STANDARD DETAIL E 201.4.0 FOR ADDITIONAL CATCH BASIN DETAILS.

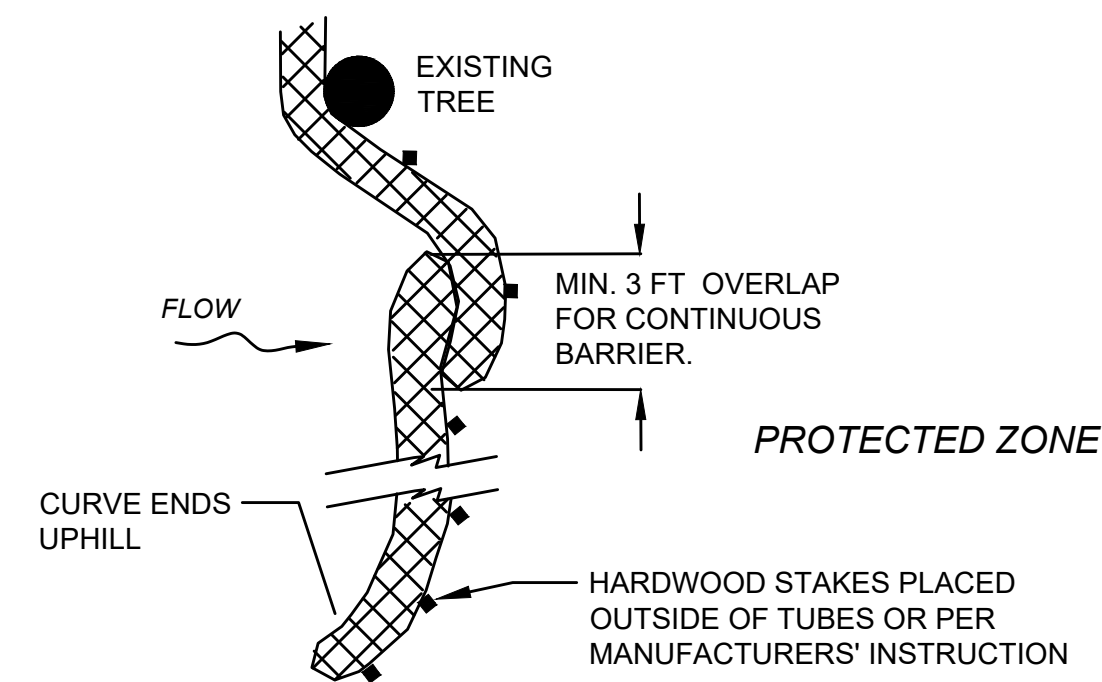
- PAVE AS SPECIFIED - SEE PAVEMENT NOTES
- NOTES:
1. COMMON FILL MATERIAL TO CONSIST OF GRANULAR MATERIAL CONTAINING NO STONES LARGER THAN 6" IN GREATEST DIMENSION.
  2. BACKFILL WITH SELECT MATERIAL CONTAINING NO STONES LARGER THAN 3" IN GREATEST DIMENSION TO 12" OVER PIPE FOR DRAIN PIPES.
  3. PROVIDE SCREENED GRAVEL BEDDING TO MID PIPE DIAMETER AND CRUSHED STONE WHERE GROUNDWATER IS ENCOUNTERED AS DIRECTED BY THE ENGINEER.
  4. REMOVE UNSUITABLE MATERIAL BELOW GRADE IF ENCOUNTERED, TO SUITABLE DEPTHS AS DIRECTED BY ENGINEER AND REPLACE WITH CLEAN GRANULAR FILL.
  5. BACKFILL SHALL BE PLACED IN LIFTS OF A MAXIMUM OF 6 INCHES
  6. ALL TRENCH CONSTRUCTION TO CONFORM TO APPLICABLE FEDERAL, STATE AND LOCAL REGULATIONS.
  7. COMPACT FILL AND TAMP TO 95% MAX. DENSITY UNLESS OTHERWISE SPECIFIED.
  8. PAYMENT FOR HMA PERMANENT TRENCH PATCHING SHALL BE PAID FOR UNDER ITEM 451. HMA FOR PATCHING.
  9. PAYMENT FOR HMA TEMPORARY TRENCH PATCHING SHALL BE PAID FOR UNDER ITEM 472. TEMPORARY ASPHALT PATCHING.



**SILT SACK**  
NOT TO SCALE



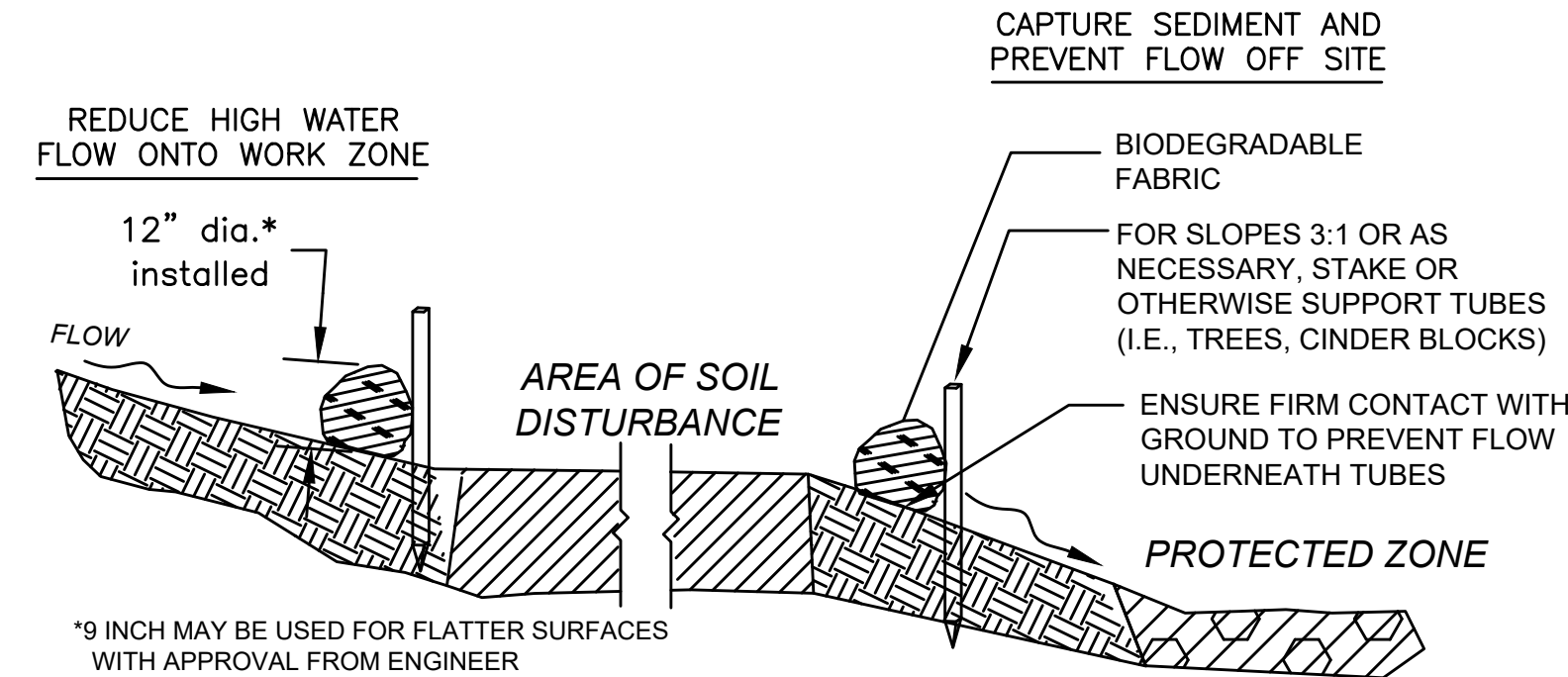
**SETTING RURAL MAILBOXES**  
NOT TO SCALE



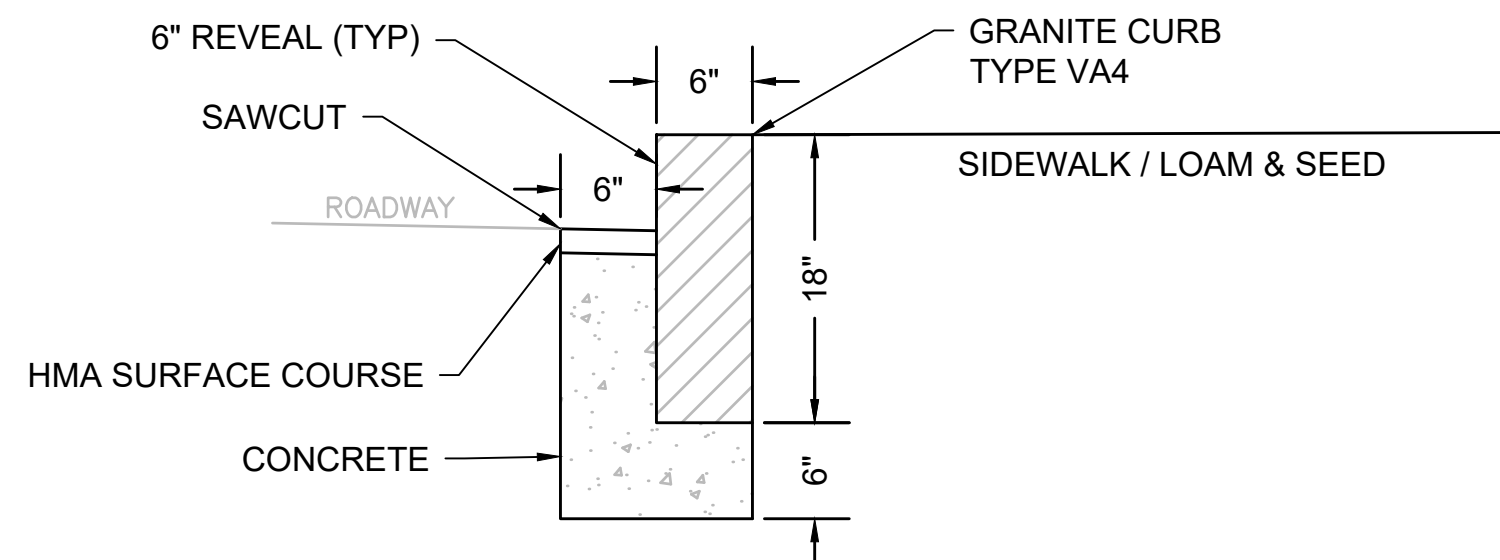
PLACE TUBE AS CLOSE TO LIMIT OF SOIL DISTURBANCE AS POSSIBLE, ALONG CONTOURS, AND PERPENDICULAR TO FLOW.

ADJUST LOCATION AS REQUIRED FOR OPTIMUM EFFECTIVENESS. DO NOT INSTALL IN WATERWAYS.

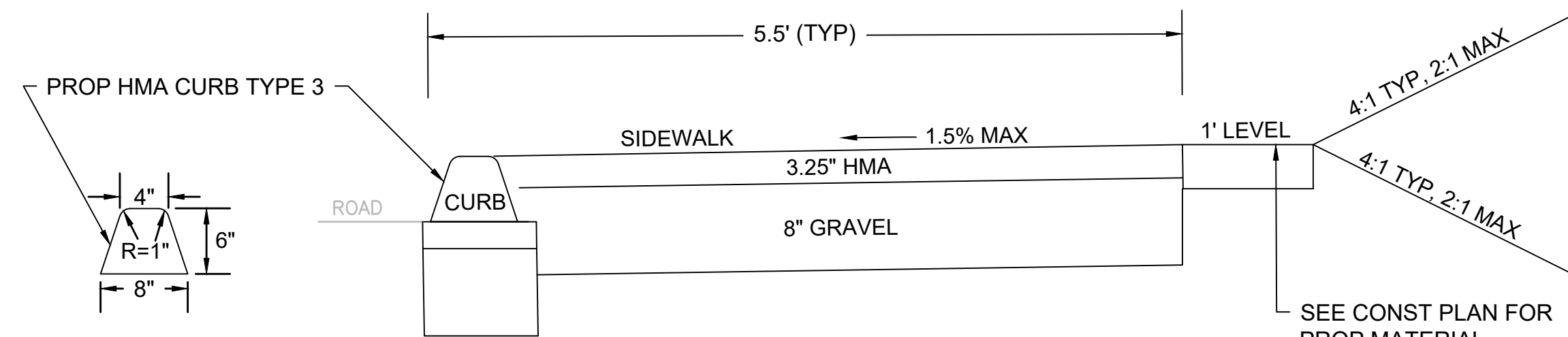
**PLAN VIEW**



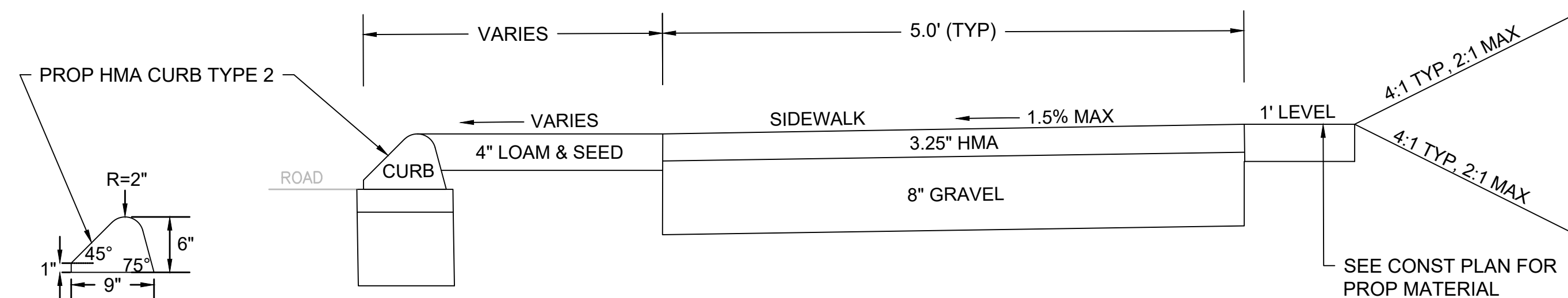
**SECTION VIEW**  
**SEDIMENTATION FENCE & COMPOST SOCK**  
NOT TO SCALE



**GRANITE CURB SETTING DETAIL**  
NOT TO SCALE



**TYPICAL SIDEWALK SECTION ADJACENT TO ROADWAY DETAIL**  
NOT TO SCALE



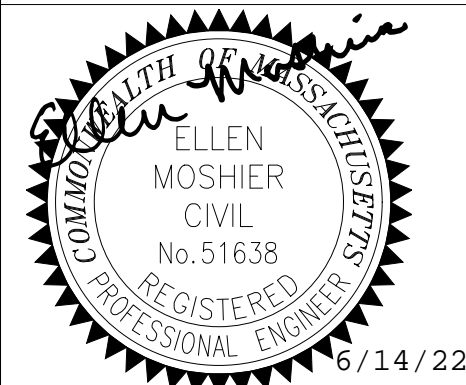
**TYPICAL SIDEWALK SECTION WITH GRASS STRIP DETAIL**  
NOT TO SCALE



**TOWN OF  
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**CONSTRUCTION  
DETAILS - 1**

No.	Submittal/ Revision	App'd. By	Date
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3	PRELIM		03/02/22
4	PRELIM - REV 1		03/09/22
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6	FINAL REV 1		06/14/22

Designed By: DC/JB	Drawn By: DC/JB	Checked By: JGM
Issue Date: 06-14-2022	Project No: 71414	Scale: NTS

Drawing No.:

**13** OF **20**



PAVEMENT NOTES

PROPOSED FULL DEPTH PERMANENT TRENCH PATCH  
3" SUPERPAVE INTERMEDIATE COURSE 19.0 (SIC-19.0) OVER  
3" SUPERPAVE INTERMEDIATE COURSE 19.0 (SIC-19.0) OVER  
12" GRAVEL BORROW TYPE b

PROPOSED TEMPORARY TRENCH PATCH  
3" SUPERPAVE INTERMEDIATE COURSE - 19.0 (SIC - 19.0) (ITEM #472) OVER  
8" GRAVEL BORROW TYPE b OVER COMPACTED TRENCH BACKFILL

PROPOSED FULL DEPTH PAVEMENT WIDENING  
1-1/2" SUPERPAVE SURFACE COURSE 9.5 (SSC-9.5) OVER  
2" SUPERPAVE INTERMEDIATE COURSE 12.5 (SIC-12.5) OVER  
6" CEMENT CONCRETE BASE COURSE OVER  
8" GRAVEL BORROW TYPE b

PROPOSED HOT MIX ASPHALT DRIVEWAYS  
1-1/2" SUPERPAVE SURFACE COURSE 9.5 (SSC-9.5) OVER  
2-1/2" SUPERPAVE SURFACE COURSE 12.5 (SSC-12.5) OVER  
8" GRAVEL BORROW TYPE b

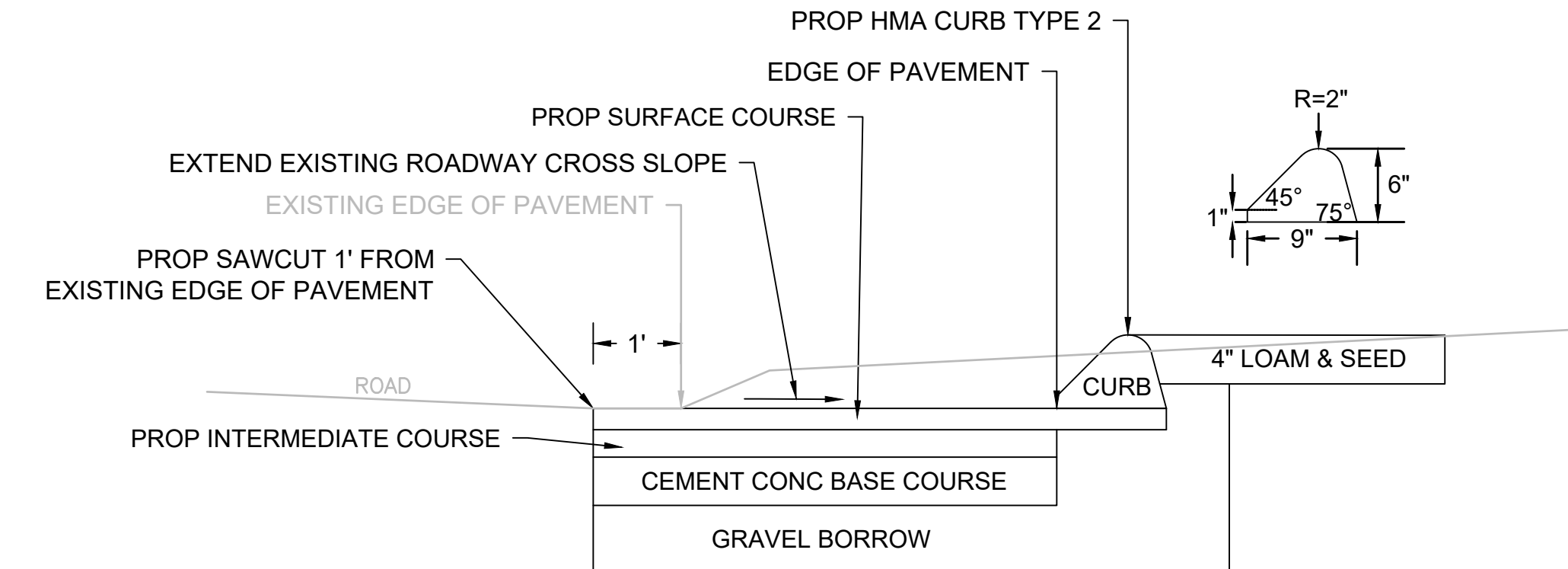
PROPOSED HOT MIX ASPHALT SIDEWALK  
1.25" SUPERPAVE SURFACE COURSE 9.5 (SSC-9.5) OVER  
2.00" SUPERPAVE SURFACE COURSE 12.5 (SSC-12.5) OVER  
8" GRAVEL BORROW - TYPE b

PROPOSED GRAVEL DRIVEWAY  
12" GRAVEL BORROW TYPE b OVER  
COMPACTED SUBGRADE

PROPOSED CEMENT CONCRETE PEDESTRIAN CURB RAMP / SIDEWALK  
6" CEMENT CONCRETE OVER  
8" GRAVEL BORROW - TYPE b

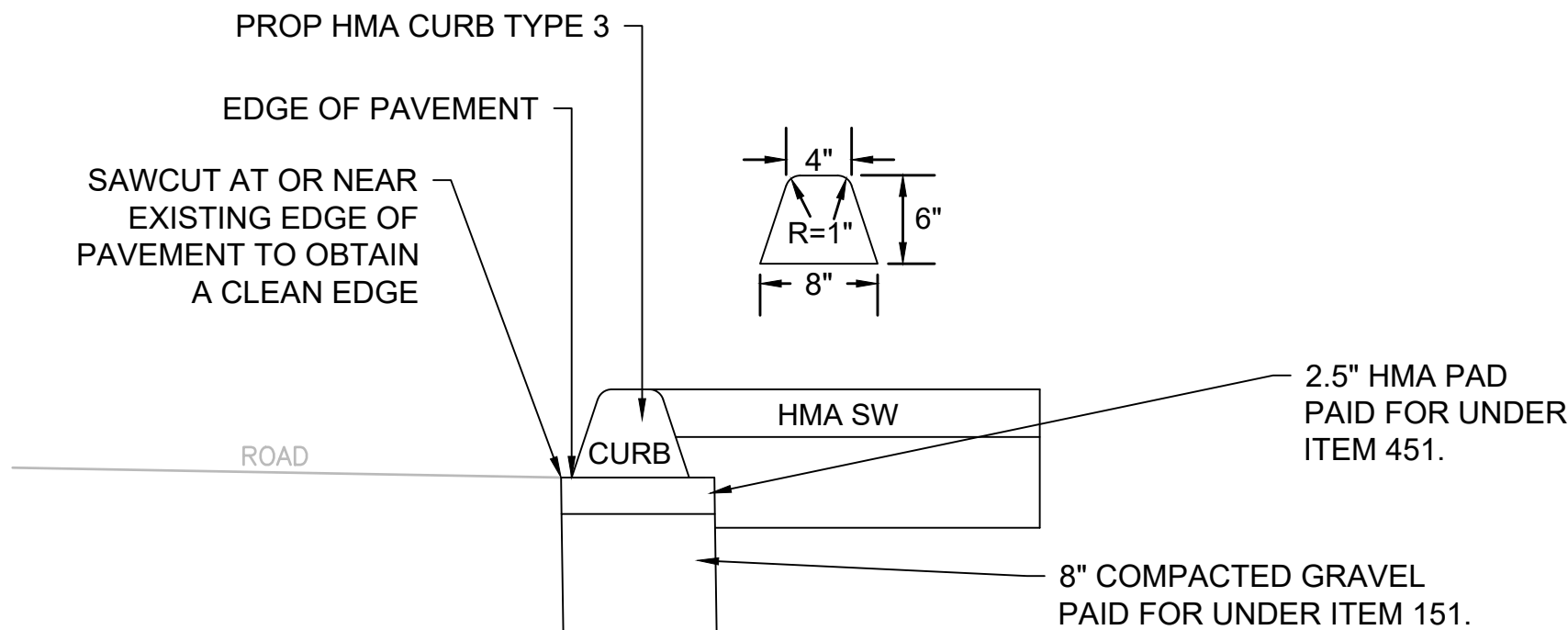
NOTES:

- TACK COAT APPLICATION RATES FOR SPECIFIC SURFACE CONDITIONS SHALL BE IN ACCORDANCE WITH THE FOLLOWING:
- ON A NEW HMA SURFACE, NOT OPENED TO TRAFFIC, THE EMULSION APPLICATION RATE SHALL EQUAL 0.06 TO 0.08 GALLONS PER SQUARE YARD
  - ON AN EXISTING TIGHT SMOOTH PAVEMENT THE EMULSION APPLICATION RATE SHALL EQUAL 0.06 TO 0.08 GALLONS PER SQUARE YARD
  - ON A MILLED SURFACE THE EMULSION APPLICATION RATE SHALL EQUAL 0.07 TO 0.09 GALLONS PER SQUARE YARD
  - ON CEMENT CONCRETE BASE COURSE THE EMULSION APPLICATION RATE SHALL BE EQUAL TO SPRAY APPLICATION FOR ADJACENT SURFACE
  - ON NEW HMA PATCHES THE EMULSION APPLICATION RATE SHALL EQUAL 0.07 TO 0.09 GALLONS PER SQUARE YARD



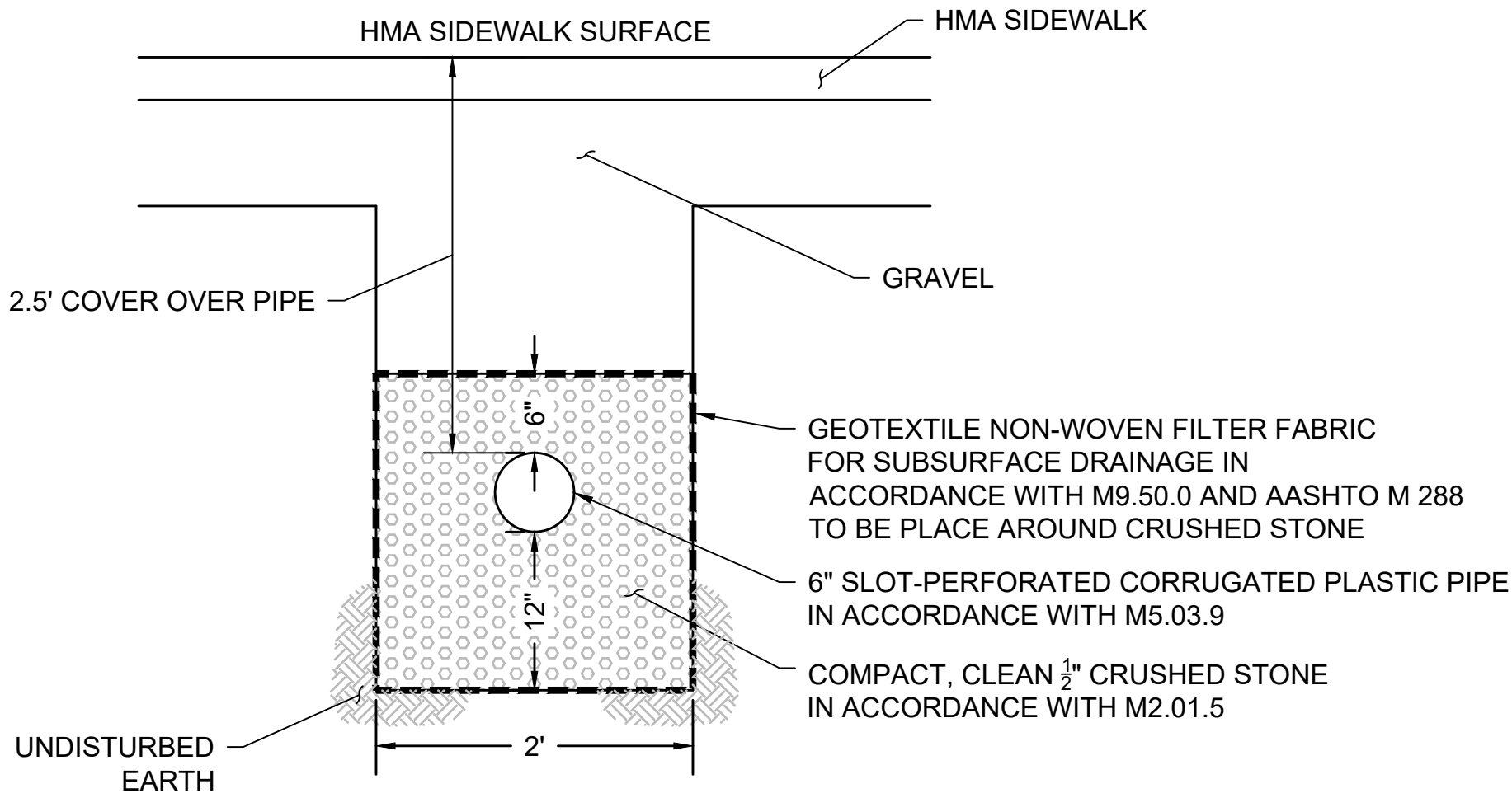
HOT MIX ASPHALT CURB TYPE 2  
ADJACENT TO FULL DEPTH PAVEMENT WIDENING

NOT TO SCALE



HOT MIX ASPHALT CURB TYPE 3  
ADJACENT TO EXISTING ROADWAY

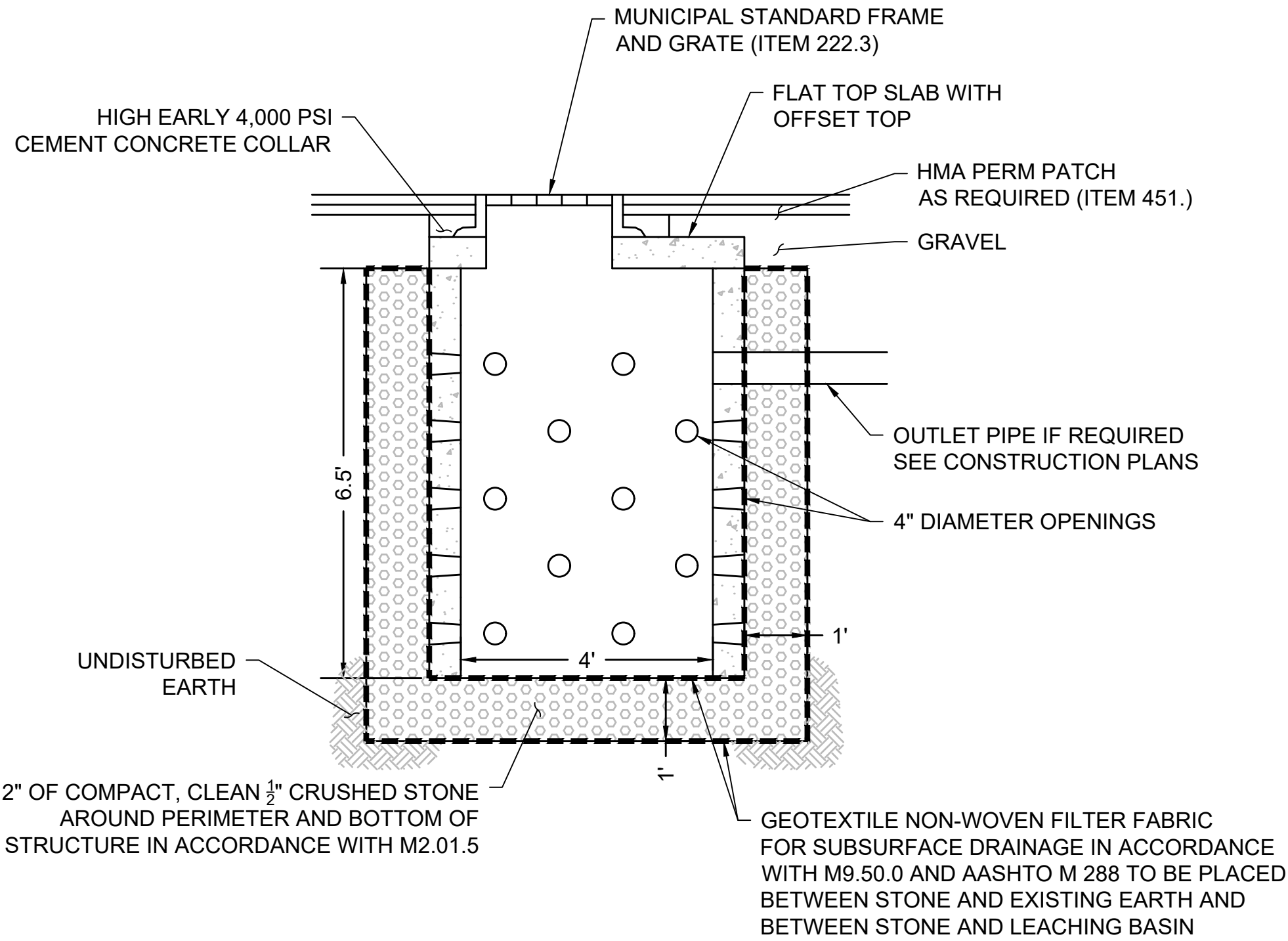
NOT TO SCALE



NOTE: SLOT-PERFORATED PIPE SHALL BE PAID FOR UNDER ITEM 269.06. PAYMENT UNDER ITEM 269.06 SHALL ALSO INCLUDE THE GEOTEXTILE FABRIC AND CRUSHED STONE REQUIRED IN ACCORDANCE WITH 260.81 OF THE STANDARD SPECIFICATIONS.

SLOT-PERFORATED PIPE TRENCH DETAIL

NOT TO SCALE



NOTE: LEACHING BASINS TO BE PAID FOR UNDER ITEM 205. LEACHING BASIN WHICH PAYMENT ALSO INCLUDES THE CRUSHED STONE, CONCRETE COLLAR, AND GEOTEXTILE FABRIC REQUIRED.

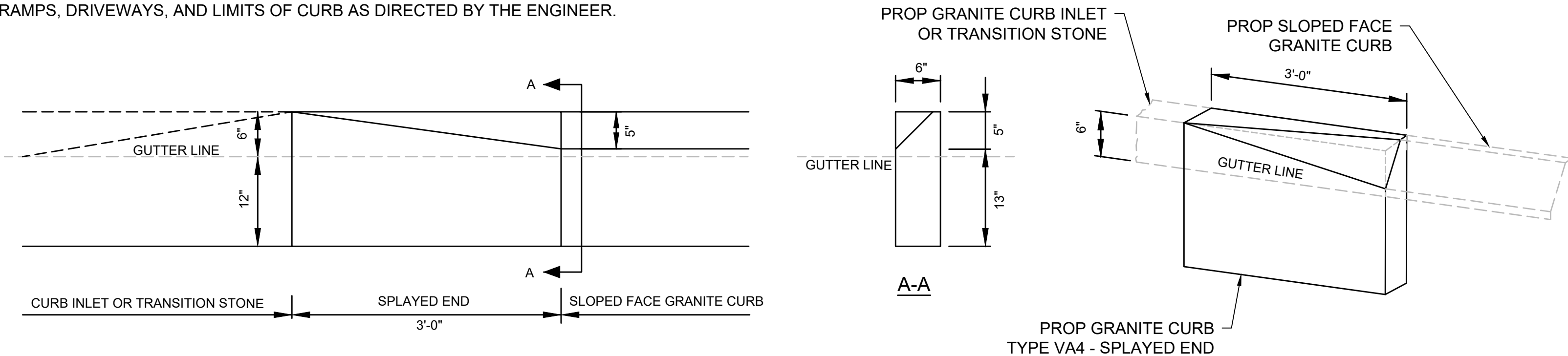
PRECAST LEACHING BASIN

NOT TO SCALE

ADD-ALTERNATIVE DETAILS

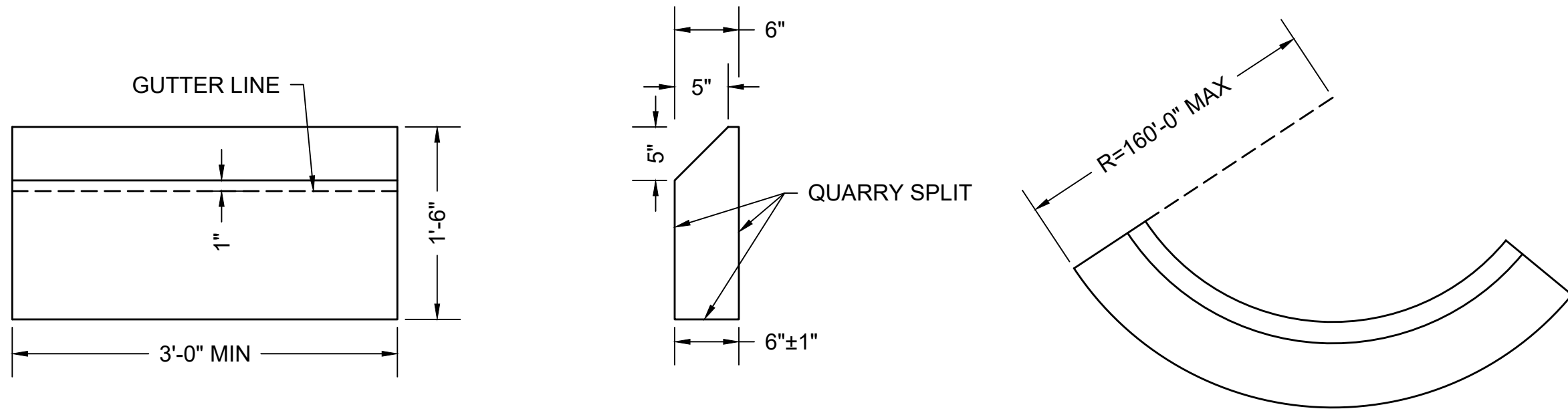
ADD-ALTERNATIVE NOTES:

- SLOPED FACE GRANITE CURB, GRANITE CURB TYPE VA4 - SPLAYED END, AND GRANITE TRANSITION CURBS FOR PEDESTRIAN RAMPS STRAIGHT AND CURVED ARE INCLUDED AS AN ADD-ALTERNATIVE IN PLACE OF THE HMA CURB TYPE 2 AND TYPE 3 AND/OR AS DIRECTED BY THE ENGINEER.
- SLOPED FACE GRANITE CURB, SPLAYED ENDS, AND GRANITE TRANSITION CURBS WILL REPLACE AREAS OF PROPOSED HMA CURB TYPE 2, HMA CURB TYPE 3, AND HMA TRANSITIONS CURBS ALONG THE FOLLOWING SECTIONS OF PROPOSED SIDEWALK:
  - LINCOLN STREET, STATION 102+21 TO STATION 104+78 LEFT
  - MIDDLE STREET, STATION 20+71 LEFT TO LINCOLN STREET STATION 121+15 RIGHT
  - LINCOLN STREET, STATION 126+29 TO STATION 128+24 RIGHT
- GRANITE SPLAYED ENDS WILL BE PROVIDED AS A TRANSITION BETWEEN THE SLOPED FACE GRANITE CURB AND CURB INLETS AND GRANITE TRANSITION STONES AT PEDESTRIAN CURB RAMPS, DRIVEWAYS, AND LIMITS OF CURB AS DIRECTED BY THE ENGINEER.



GRANITE CURB TYPE VA4 - SPLAYED END DETAIL  
(TRANSITION FROM CURB INLET & TRANSITION STONES TO  
SLOPED FACE GRANITE CURB)

NOT TO SCALE



NOTES:

- TOP SURFACE AND SLOPED SURFACE TO BE DRESSED BY SAW. REMAINDER TO BE QUARRY SPLIT.
- MINIMUM LENGTH OF STRAIGHT OR CIRCULAR PIECES TO BE 3'-0\".
- CIRCULAR CURB IS REQUIRED ON CURVES WITH RADII OF 160'-0\" OR LESS. STRAIGHT CURB TO BE USED ON CURVES OF MORE THAN 160'-0\" RADIUS.

SLOPED FACE GRANITE CURB DETAIL

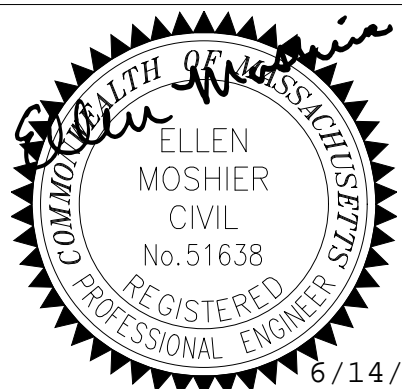
NOT TO SCALE



TOWN OF  
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CONSTRUCTION  
DETAILS - 2

No.	Submittal / Revision	App'd. By	Date
1	CONCEPT		10/22/21
2	CONCEPT - REV 1		01/05/22
3	PRELIM		03/02/22
4	PRELIM - REV 1		03/09/22
5	FINAL		06/02/22
6	FINAL REV 1		06/14/22

Designed By: DC/JB	Drawn By: DC/JB	Checked By: JGM
Issue Date: 06-14-2022	Project No: 71414	Scale: NTS

Drawing No.:

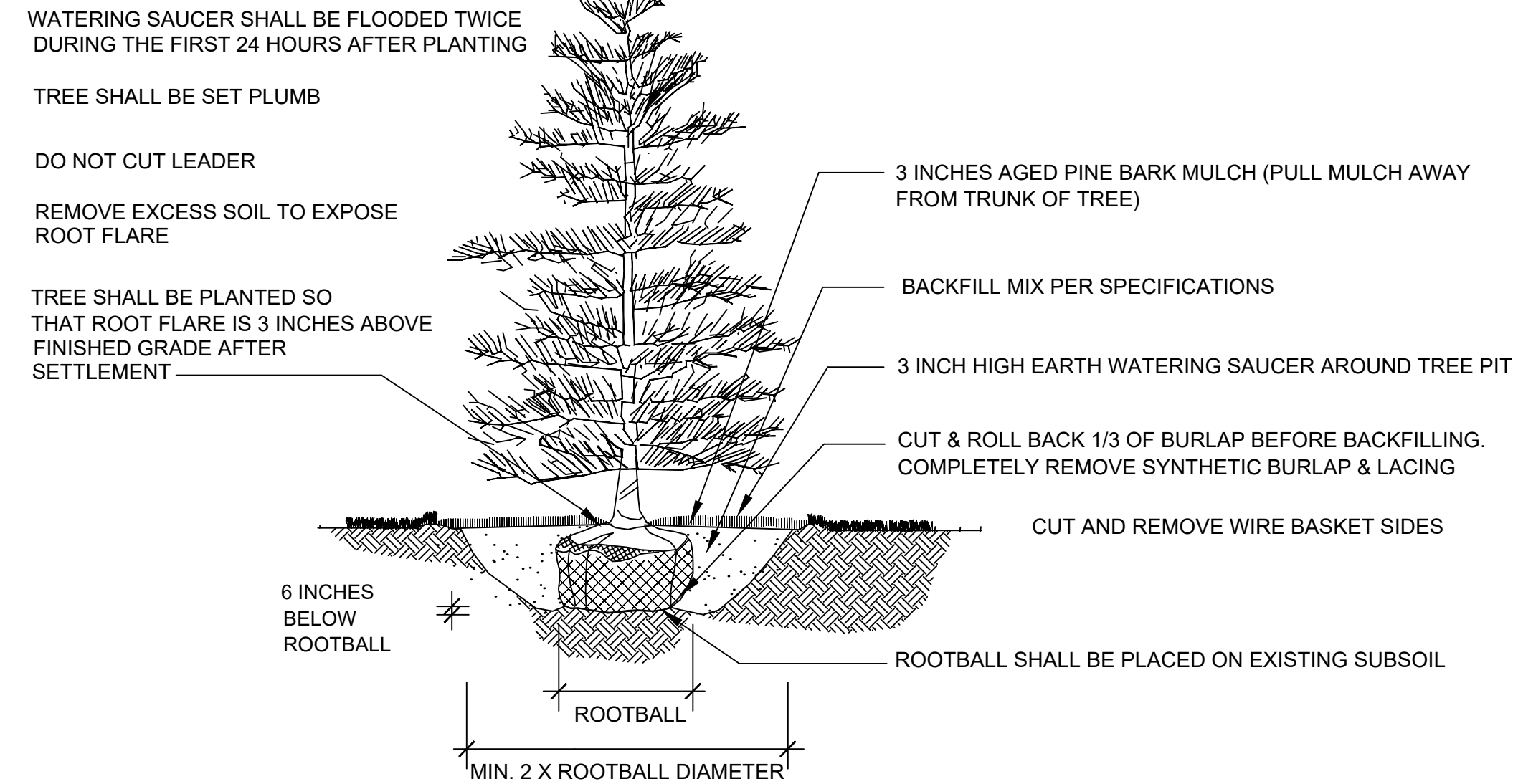
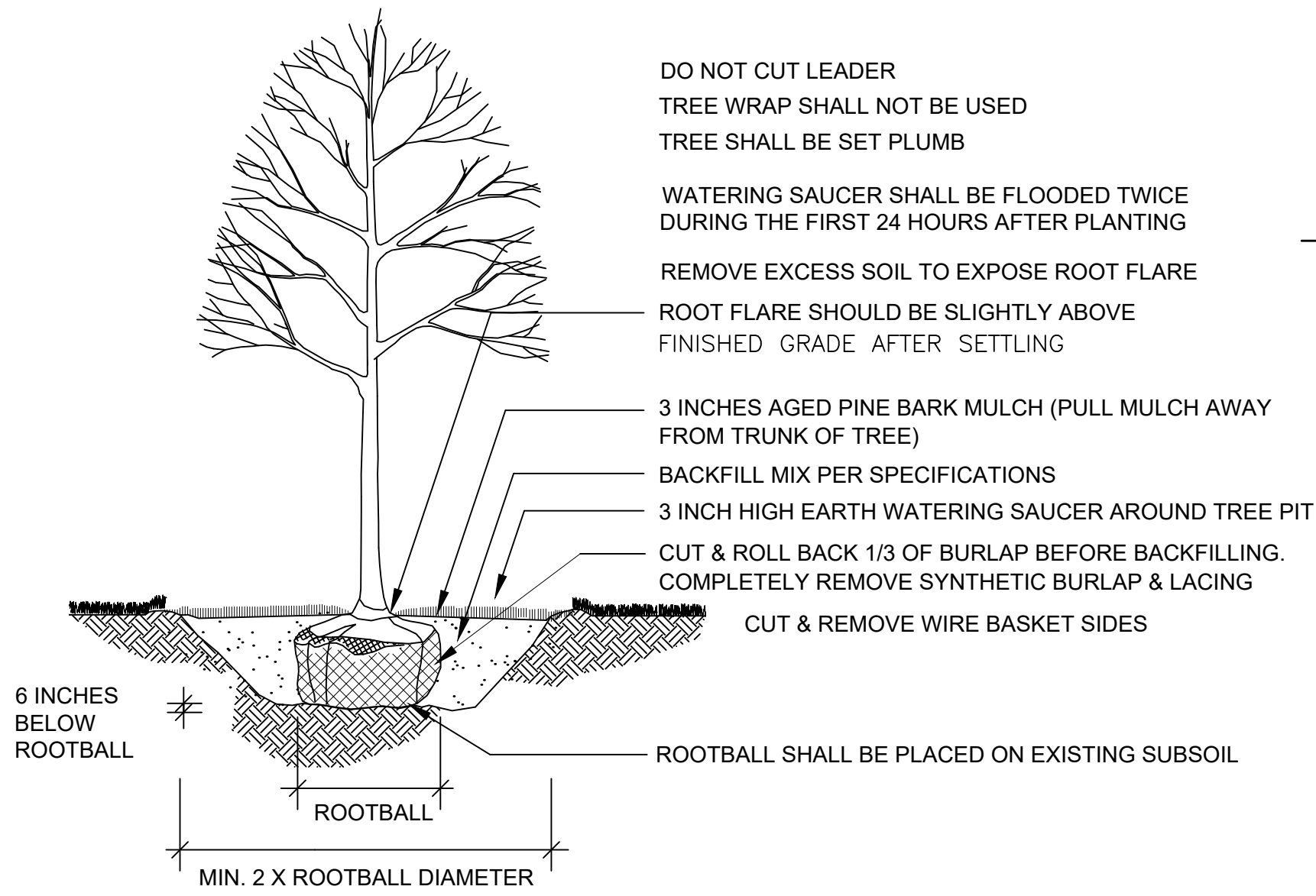
14 OF 20



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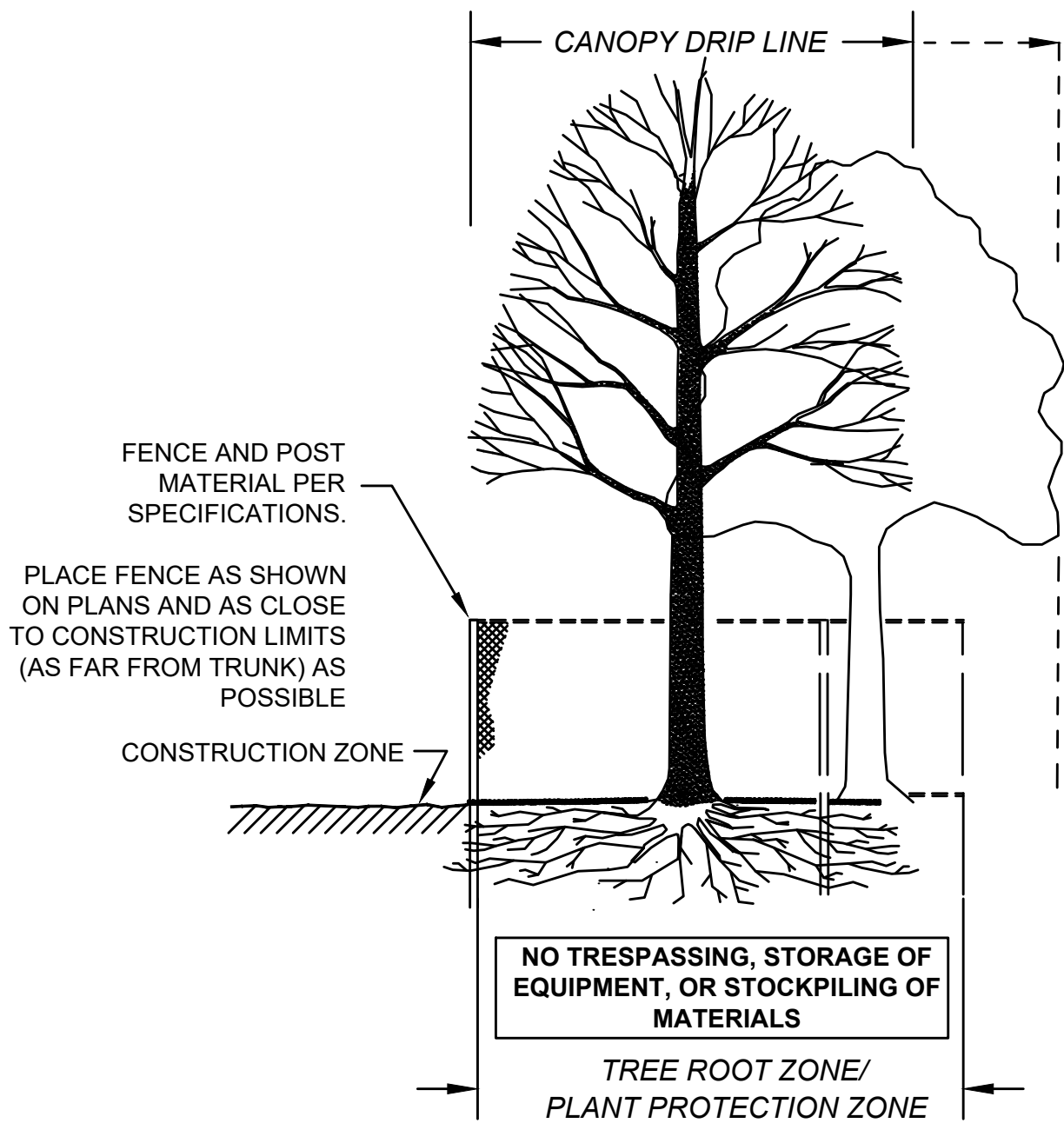
## DECIDUOUS TREE PLANTING

NOT TO SCALE

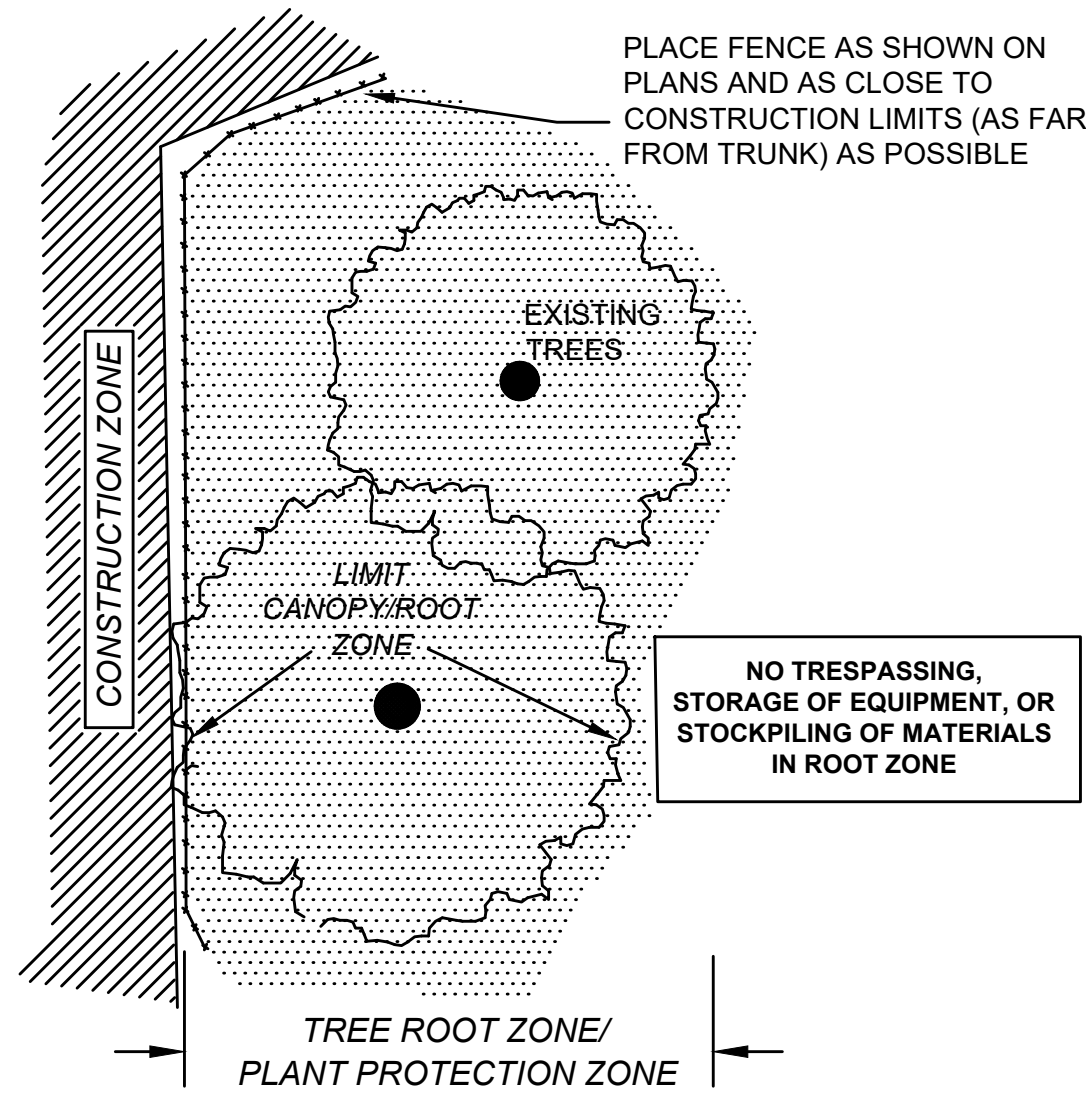


## EVERGREEN TREE PLANTING

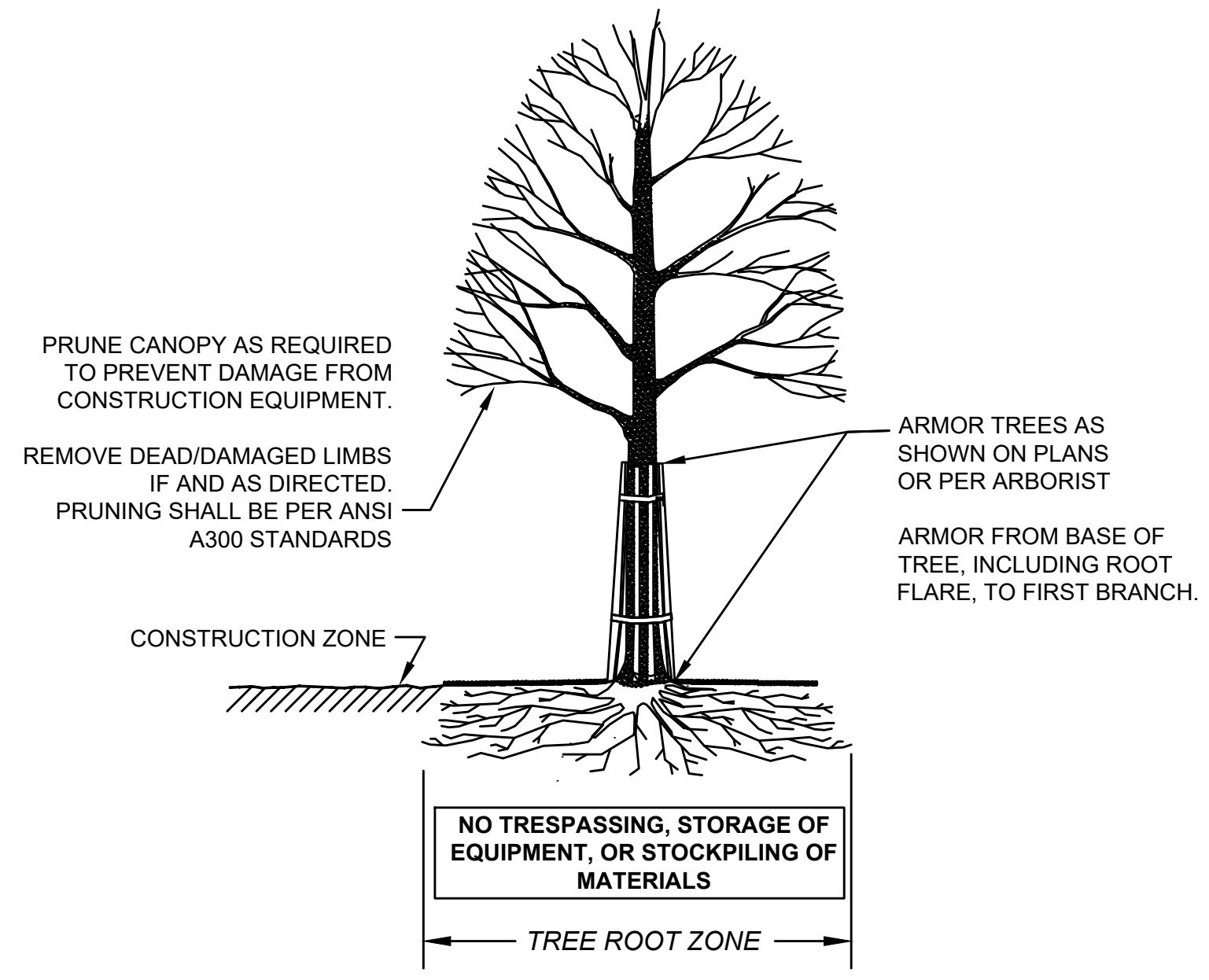
NOT TO SCALE



SECTION - FENCE PROTECTION OF ROOT ZONE



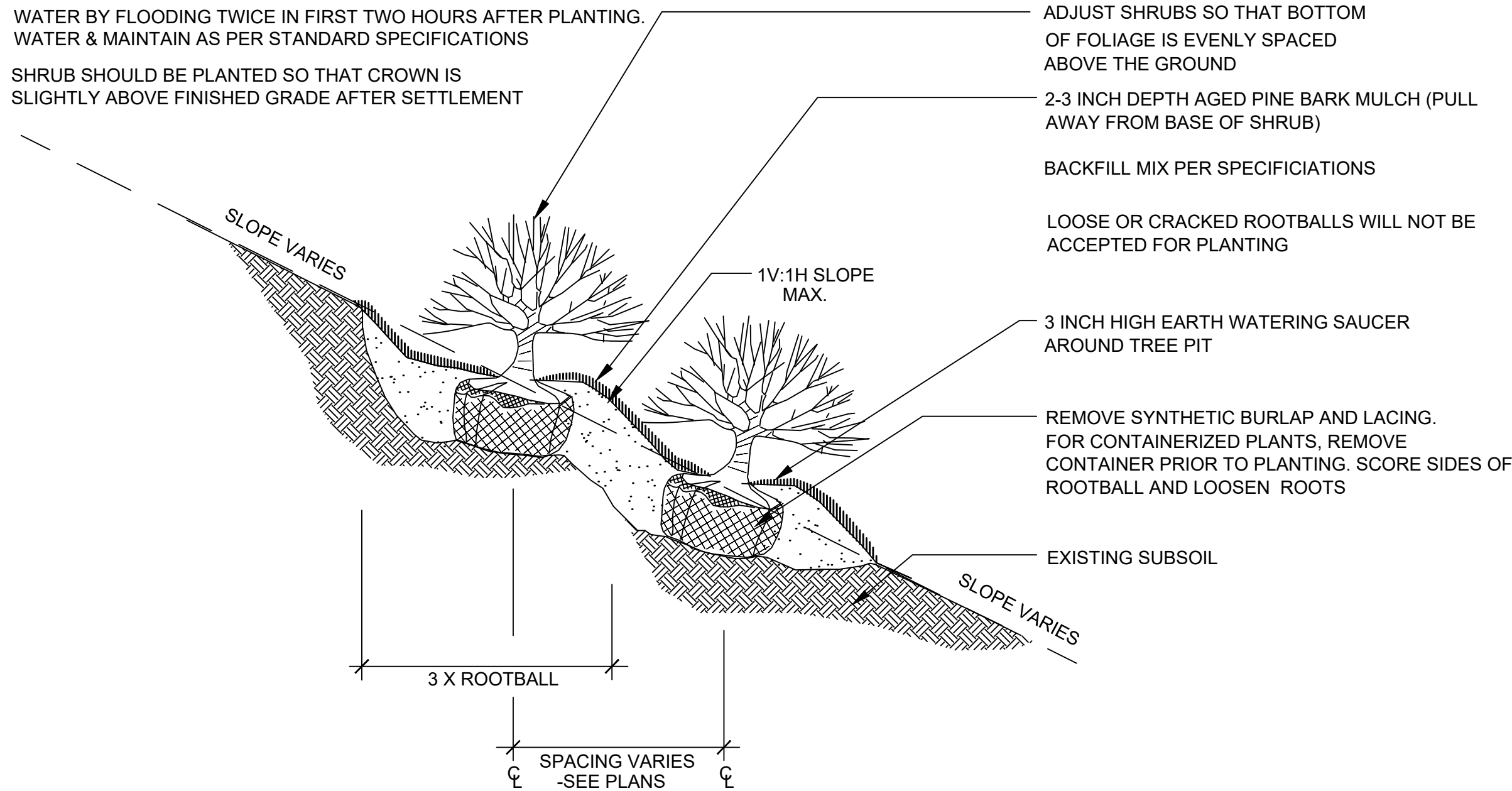
PLAN VIEW - FENCE PROTECTION OF ROOT ZONE



SECTION - TRUNK ARMORING & PRUNING

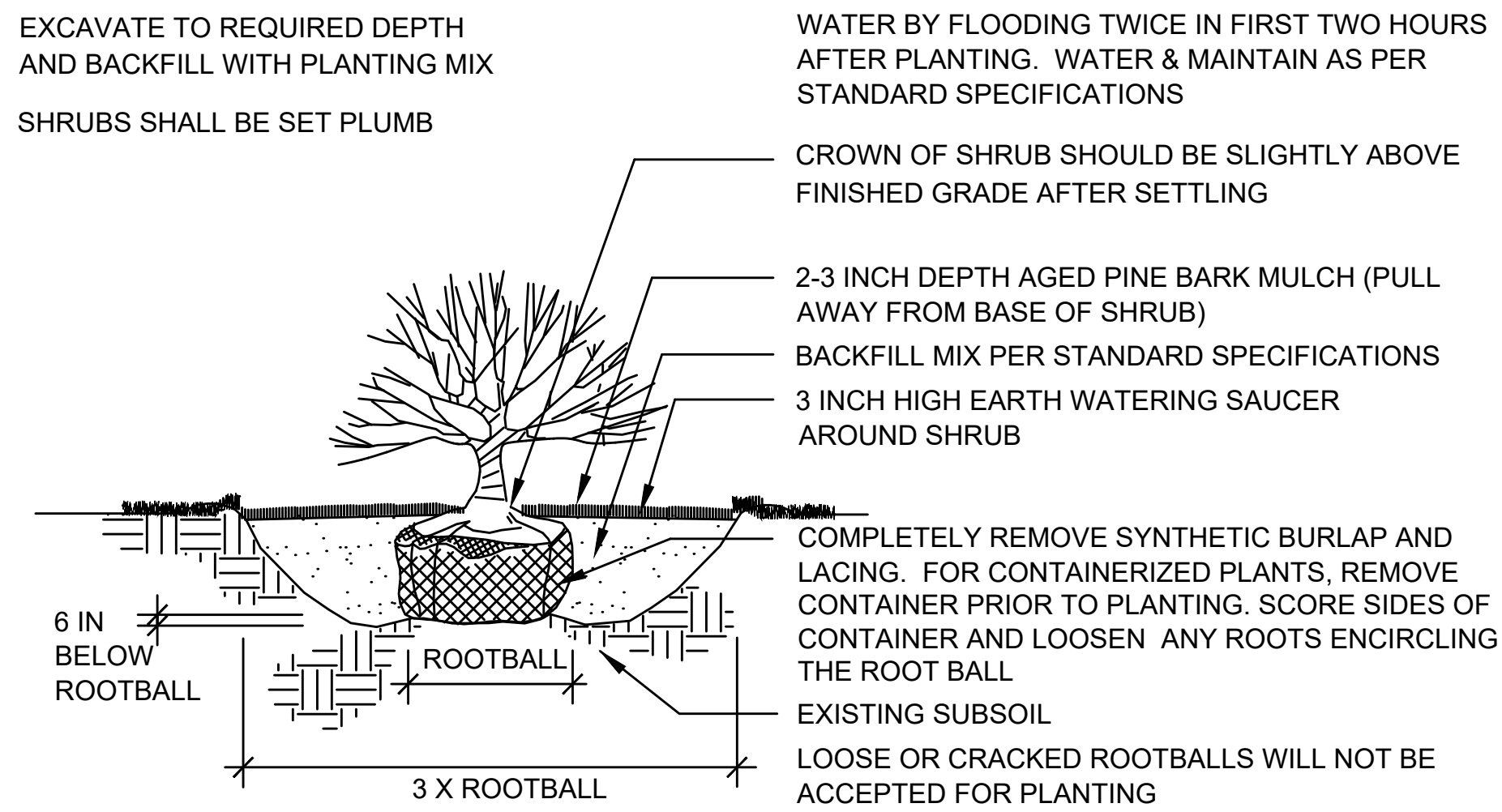
## TREE PROTECTION DETAIL

NOT TO SCALE



## CONTAINERIZED SHRUB PLANTING (SLOPE) DETAIL

NOT TO SCALE



## SHRUB PLANTING

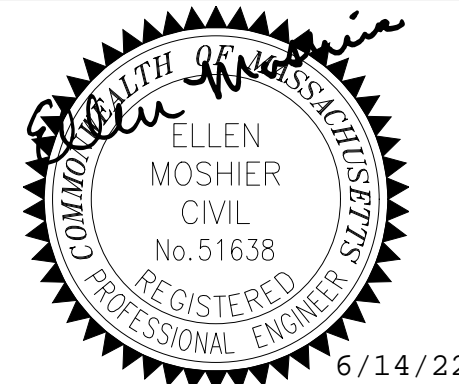
NOT TO SCALE



TOWN OF  
LEXINGTON,  
MASSACHUSETTS

HIGHWAY  
DEPARTMENT

LINCOLN STREET  
SIDEWALK  
PROJECT



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## CONSTRUCTION DETAILS - 3

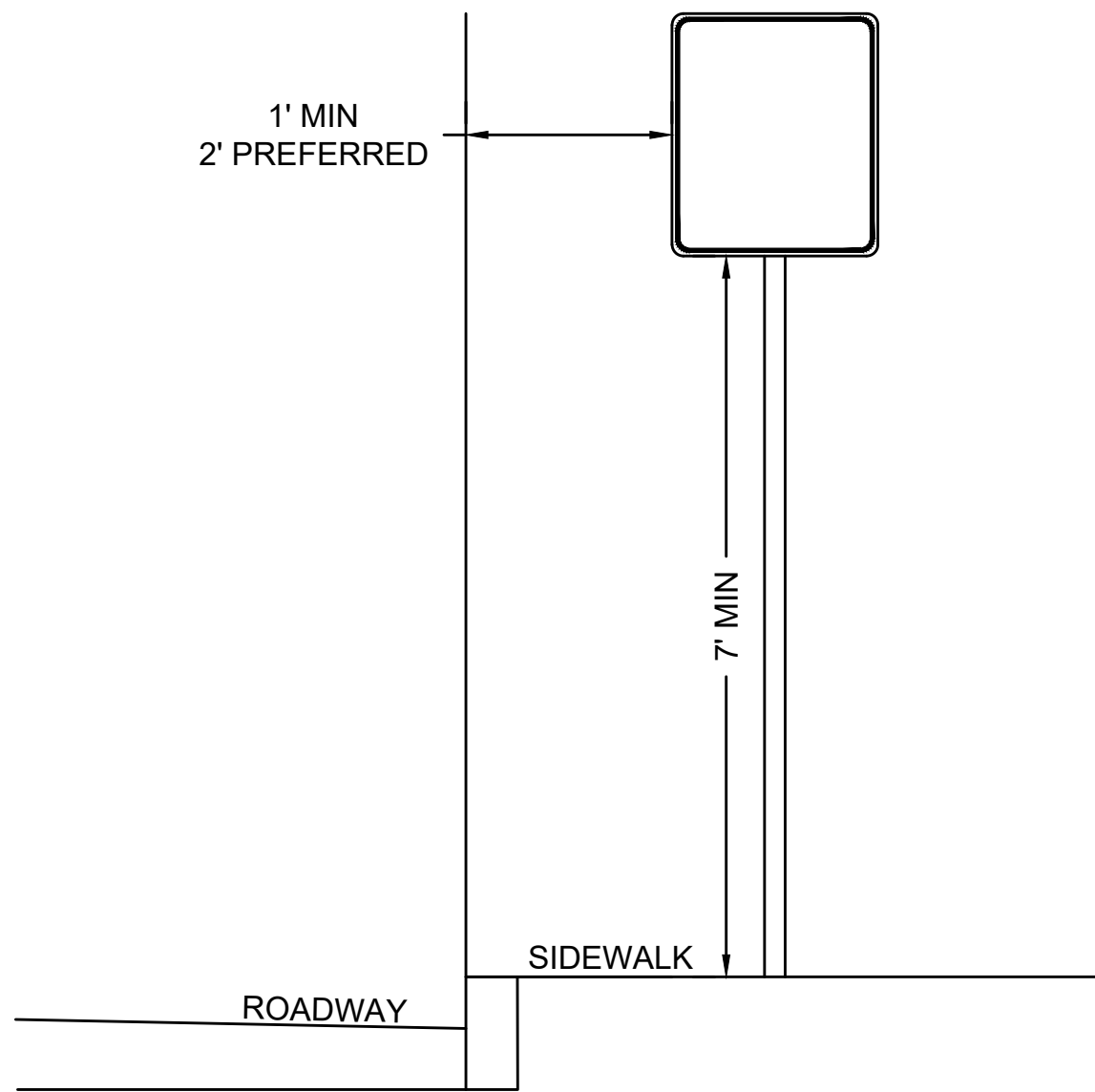
No.	Submittal / Revision	App'd.	By	Date
1	CONCEPT			10/22/21
2	CONCEPT - REV 1			01/05/22
3	PRELIM			03/02/22
4	PRELIM - REV 1			03/09/22
5	FINAL			06/02/22
6	FINAL REV 1			06/14/22

Designed By: DC/JB	Drawn By: DC/JB	Checked By: JGM
Issue Date: 06-14-2022	Project No: 71414	Scale: NTS

Drawing No.:

15 OF 20

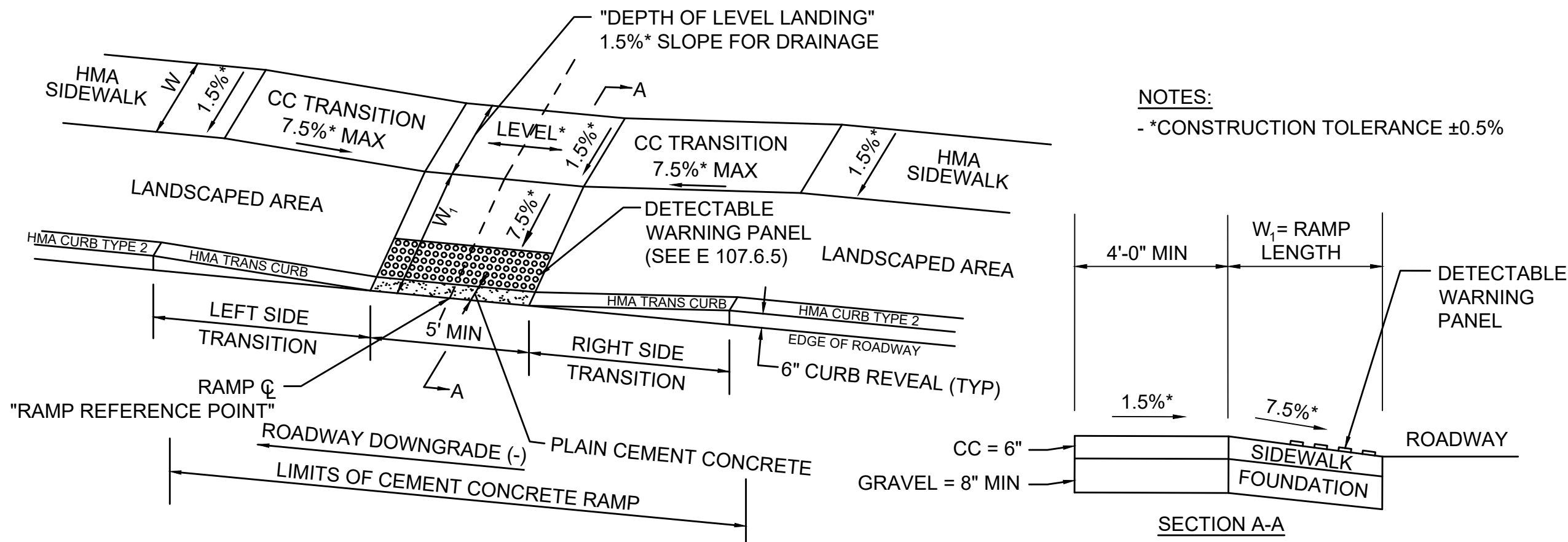




**SIGN HEIGHT & LATERAL OFFSET DETAIL**  
NOT TO SCALE

**GENERAL PEDESTRIAN CURB RAMP NOTES:**

1. ALL PEDESTRIAN CURB RAMPS SHALL CONFORM TO THE REQUIREMENTS OF THE MASSACHUSETTS ARCHITECTURAL ACCESS BOARD (AAB), THE AMERICAN WITH DISABILITIES ACT (ADA) AND THE LATEST MASSDOT HIGHWAY DIVISION PEDESTRIAN CURB RAMP STANDARDS.
2. THE LOCATIONS OF THE PROPOSED PEDESTRIAN CURB RAMPS ARE SHOWN ON THE CONSTRUCTION PLANS. ADDITIONAL TYPICAL DETAILS AND PEDESTRIAN CURB RAMP DATA TABLE ARE SHOWN IN THE CONSTRUCTION DETAILS.
3. PROPOSED PEDESTRIAN CURB RAMP SLOPES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO THE PLACING OF CONCRETE, AND BE ADJUSTED, IF NECESSARY, TO CONFORM TO THE LATEST ADAAG/ PROWAG/ MAAB STANDARDS, AS DIRECTED BY THE ENGINEER.
4. ALL PROPOSED CURB FOR PEDESTRIAN CURB RAMP TRANSITIONS SHALL BE CUT AND TRANSITIONED AS NECESSARY TO PROVIDE THE CORRECT TRANSITION LENGTHS FOR EACH PEDESTRIAN CURB RAMP, AS SHOWN ON THE PEDESTRIAN CURB RAMP DETAILS OR AS DIRECTED BY THE ENGINEER. ANY EXISTING CURB INLETS IN AREAS OF NEW PEDESTRIAN CURB RAMP TRANSITIONS SHALL BE REMOVED AND REPLACED WITH APPROPRIATE TRANSITION CURB AS DIRECTED BY THE ENGINEER.
5. IN NO CASE, EXCEPT FOR MAXIMUM LENGTH HIGH SIDE TRANSITIONS (GUTTER SLOPES GREATER THAN 4%) SHALL ANY TRANSITION SLOPE OF ANY PEDESTRIAN CURB RAMP EXCEED 7.5%.
6. WHEN PLACEMENT OF THE PROPOSED PEDESTRIAN CURB RAMP IS SUCH THAT IT IS UNAVOIDABLE FOR EXISTING STRUCTURES, THAT CANNOT BE MOVED OR RESET, TO BE WITHIN THE PEDESTRIAN CURB RAMP, EXTREME CARE SHALL BE TAKEN SUCH THAT THE EXISTING STRUCTURE IS WITHIN THE TRANSITIONS ONLY, IF POSSIBLE, WHEN THE NEW PEDESTRIAN CURB RAMP IS PLACED AND THAT A 36 INCH WIDE (MIN) CLEAR PATH OF TRAVEL EXISTS BETWEEN THE EXISTING STRUCTURE AND EITHER THE CURBLINE OR THE BACK OF THE SIDEWALK, AS DIRECTED BY THE ENGINEER.
7. IN RARE INSTANCES WHERE AN EXISTING MANHOLE, HANDHOLE OR OTHER EXISTING "SURFACE" TYPE STRUCTURE WHICH CANNOT BE REMOVED OR RESET, IS WITHIN THE ACTUAL PEDESTRIAN CURB RAMP PATH, THE STRUCTURE SHALL BE CAREFULLY ADJUSTED SUCH THAT THE TOPMOST SURFACE OR THE STRUCTURE COVER SHALL BE FLUSH WITH THE NEW RAMP SURFACE AND SHALL MATCH THE SLOPE OF THE NEW PEDESTRIAN CURB RAMP EXACTLY, AS DIRECTED BY THE ENGINEER.
8. ALL PEDESTRIAN CURB RAMPS WITHIN THE PROJECT LIMITS SHALL HAVE DETECTABLE WARNING PANELS INSTALLED IN ACCORDANCE WITH MASSDOT STANDARD DRAWING E 107.6.5R
9. TOLERANCE FOR CONSTRUCTION IS +/- 0.5%.



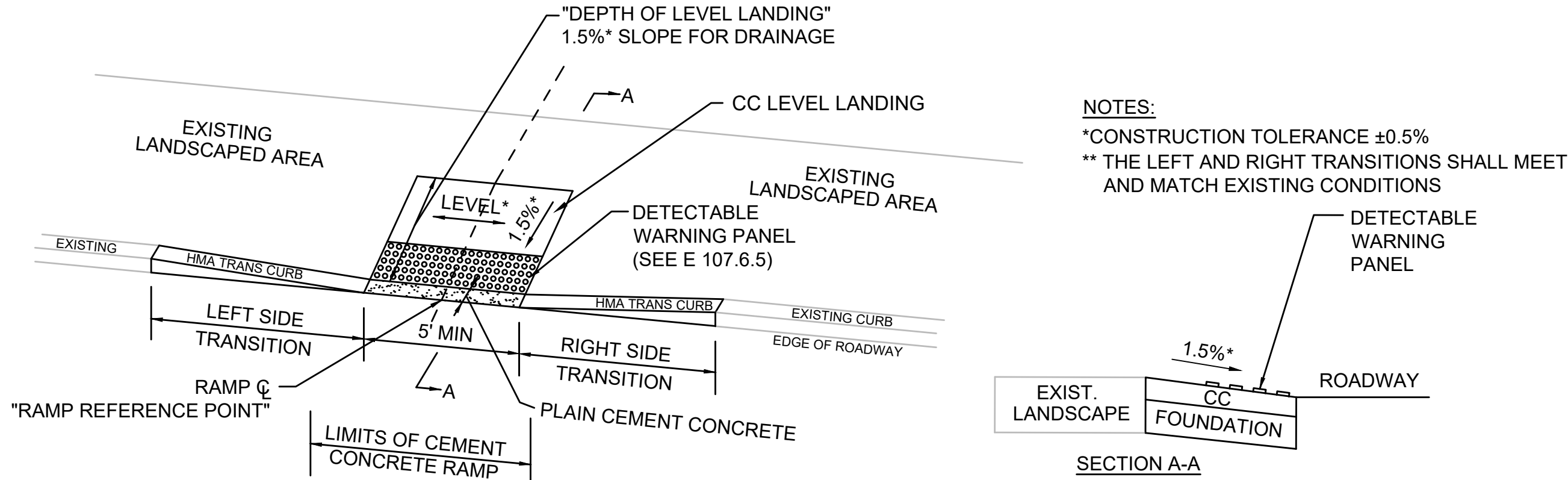
PCR #	ALIGNMENT	RAMP REFERENCE POINT		LENGTH OF PRIMARY RAMP (W1)	WIDTH OF SIDEWALK (W)	WIDTH OF RAMP OPENING MIN 5.00'	DEPTH OF LEVEL LANDING MIN 4.0'	ROADWAY GUTTER SLOPE	TRANSITION LENGTH	
		STATION	OFFSET						LEFT	RIGHT
7	MIDDLE ST	20+26.02	16.20' LT	3.00'	5.00'	5.00'	5.00'	-8.25%	6'-6"	15'-0"

**PEDESTRIAN CURB RAMP WITH LANDSCAPING STRIP & SIDEWALK TRANSITIONS**  
N.T.S.

**TRAFFIC SIGN SUMMARY**

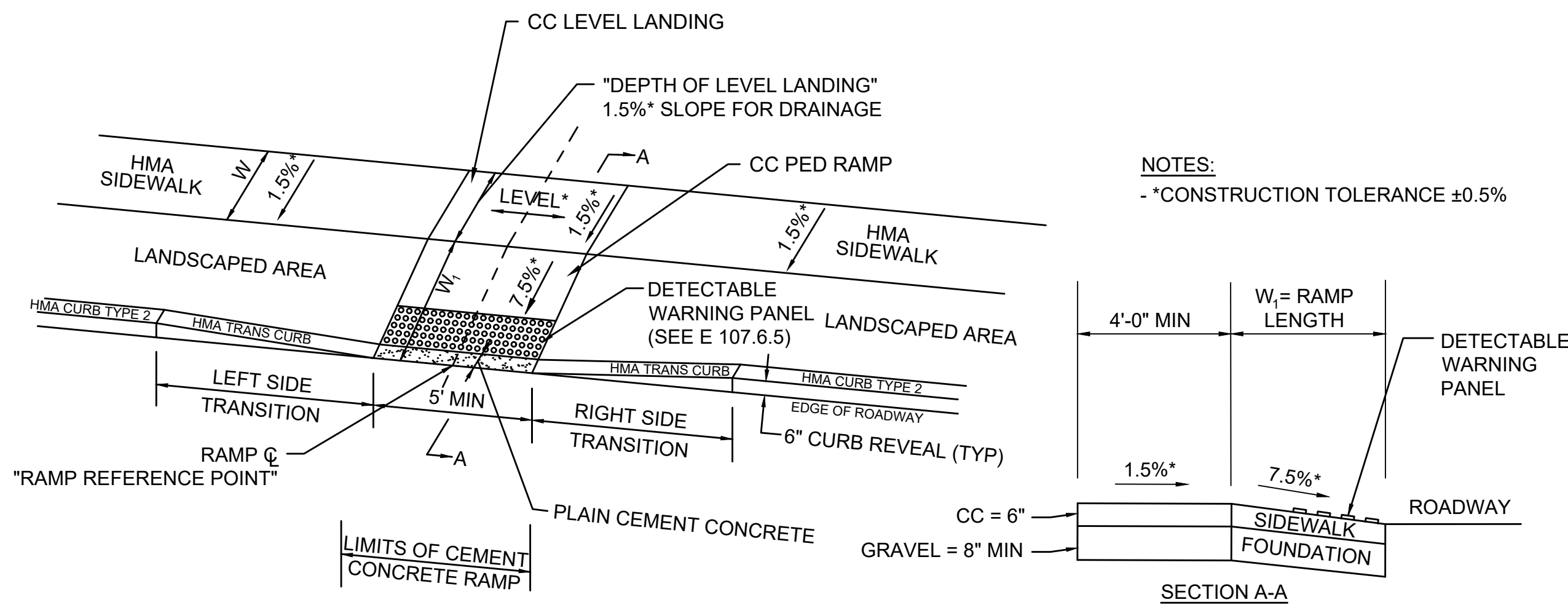
IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS			NUMBER OF SIGNS REQUIRED	COLOR			POST SIZE & NUMBER REQUIRED	UNIT AREA (SF)	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW		BACKGROUND	LEGEND	BORDER			
R1-1	30"	30"	STOP	SEE 2009 MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS			2	RED	WHITE	WHITE	P5 2	6.25	12.50
W11-2	30"	30"	WALK				16	FLUORESCENT YELLOW/GREEN	BLACK	BLACK	P5 8*	6.25	100
W16-7pL	24"	12"	LEFT TURN				8	FLUORESCENT YELLOW/GREEN	BLACK	BLACK	MOUNT W/ W11-2	2.00	16
W16-7pR	24"	12"	RIGHT TURN				8	FLUORESCENT YELLOW/GREEN	BLACK	BLACK	MOUNT W/ W11-2	2.00	16

\*INSTALL MATCHING FLUORESCENT YELLOW/GREEN REFLECTORIZED STRIP ON BOTH SIDES OF THE SIGN POST



PCR #	ALIGNMENT	RAMP REFERENCE POINT		WIDTH OF RAMP OPENING MIN 5.00'	DEPTH OF LEVEL LANDING	TRANSITION LENGTH	
		STATION	OFFSET			LEFT**	RIGHT**
15	WESTON ST	11+46.81	19.54' LT	5.00'	5.00'	2'-0"	6'-0"

**PCR #15 DETAIL**  
N.T.S.



PCR #	ALIGNMENT	RAMP REFERENCE POINT		LENGTH OF PRIMARY RAMP (W1)	WIDTH OF SIDEWALK (W)	WIDTH OF RAMP OPENING MIN 5.00'	DEPTH OF LEVEL LANDING MIN 4.0'	TRANSITION LENGTH	
		STATION	OFFSET					LEFT	RIGHT
8	LINCOLN ST	107+01.34	9.84' RT	6.92'	5.00'	5.00'	5.00'	0'-0"	6'-0"
9	LINCOLN ST	107+04.89	12.92' LT	6.36'	5.00'	5.00'	5.00'	6'-0"	6'-0"
13	BIRCH HILL LN	30+30.36	14.86' RT	3.50'	5.00'	5.00'	4.00'	6'-0"	6'-0"

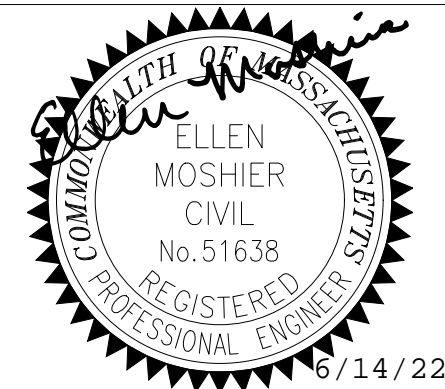
**PEDESTRIAN CURB RAMP WITH LANDSCAPING STRIP**  
N.T.S.



**TOWN OF LEXINGTON, MASSACHUSETTS**

**HIGHWAY DEPARTMENT**

**LINCOLN STREET SIDEWALK PROJECT**



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**CONSTRUCTION DETAILS - 4**

No.	Submittal / Revision	App'd. By	Date
1	CONCEPT		10/22/21
2	CONCEPT - REV 1		01/05/22
3	PRELIM		03/02/22
4	PRELIM - REV 1		03/09/22
5	FINAL		06/02/22
6	FINAL REV 1		06/14/22

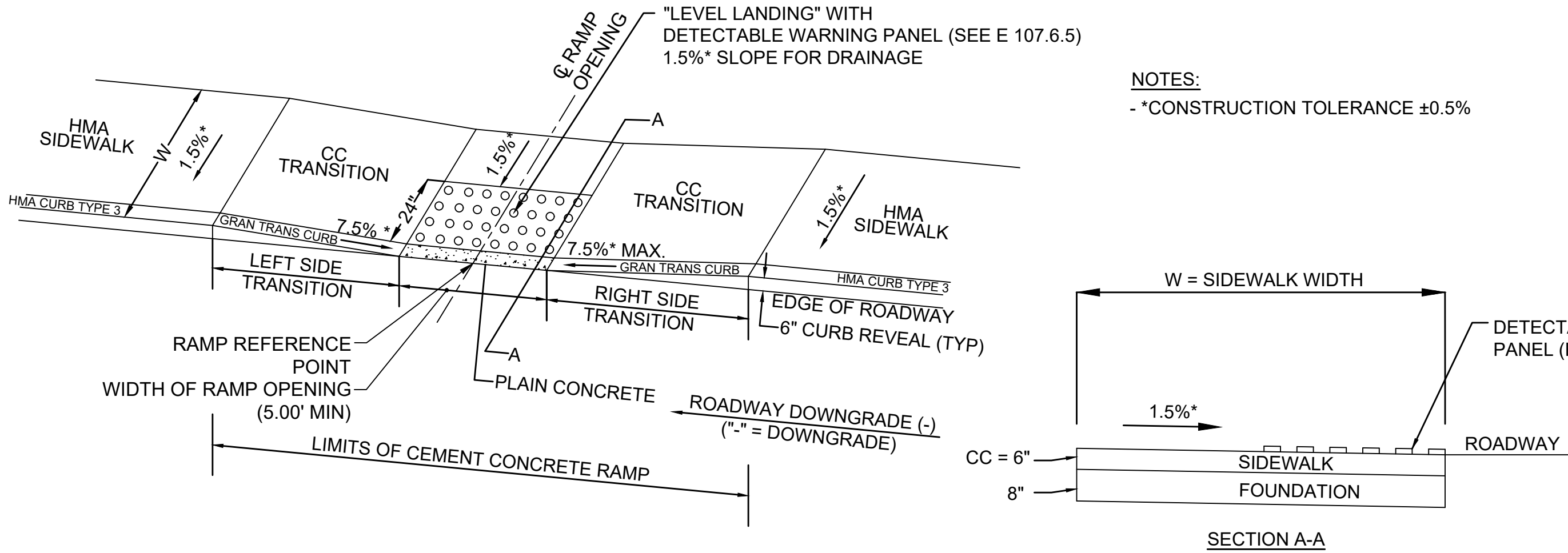
Designed By: DC/UB	Drawn By: DC/UB	Checked By: JGM
Issue Date: 06-14-2022	Project No: 71414	Scale: NTS

Drawing No.:

**16** OF **20**



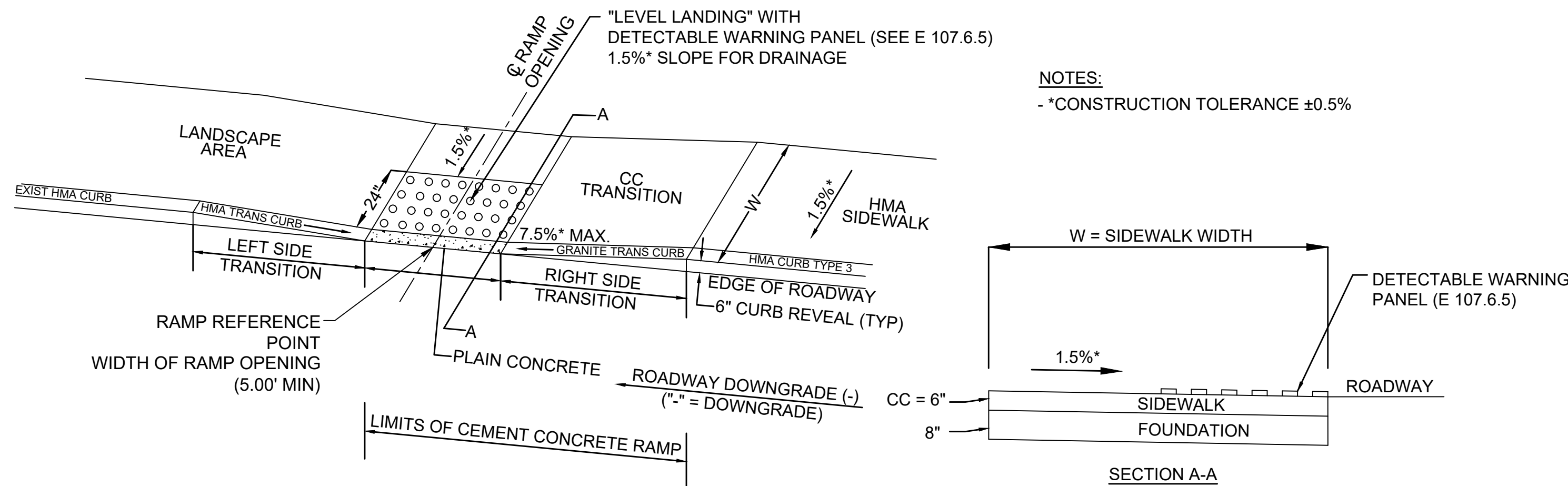
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Saved: 6/10/2022 4:46:19 PM Plotted: 6/13/2022 4:53:47 PM Current User: Chander, Daren Last Saved By: 6193



PCR #	ALIGNMENT	RAMP REFERENCE POINT		WIDTH OF SIDEWALK (W)	WIDTH OF RAMP OPENING MIN 5.00'	ROADWAY GUTTER SLOPE	TRANSITION LENGTH	
		STATION	OFFSET				LEFT	RIGHT
10	LINCOLN ST	111+65.52	8.59' RT	5.50'	5.00'	2.75%	11'-0"	6'-6"
14**	LINCOLN ST	128+34.55	17.07' RT	5.50'	5.00'	-1.07%	6'-6"	9'-0"

\*\*PROVIDE A 4'x4' LEVEL LANDING AT BOTTOM OF CURB CUT WITHIN THE MARKED CROSSWALK

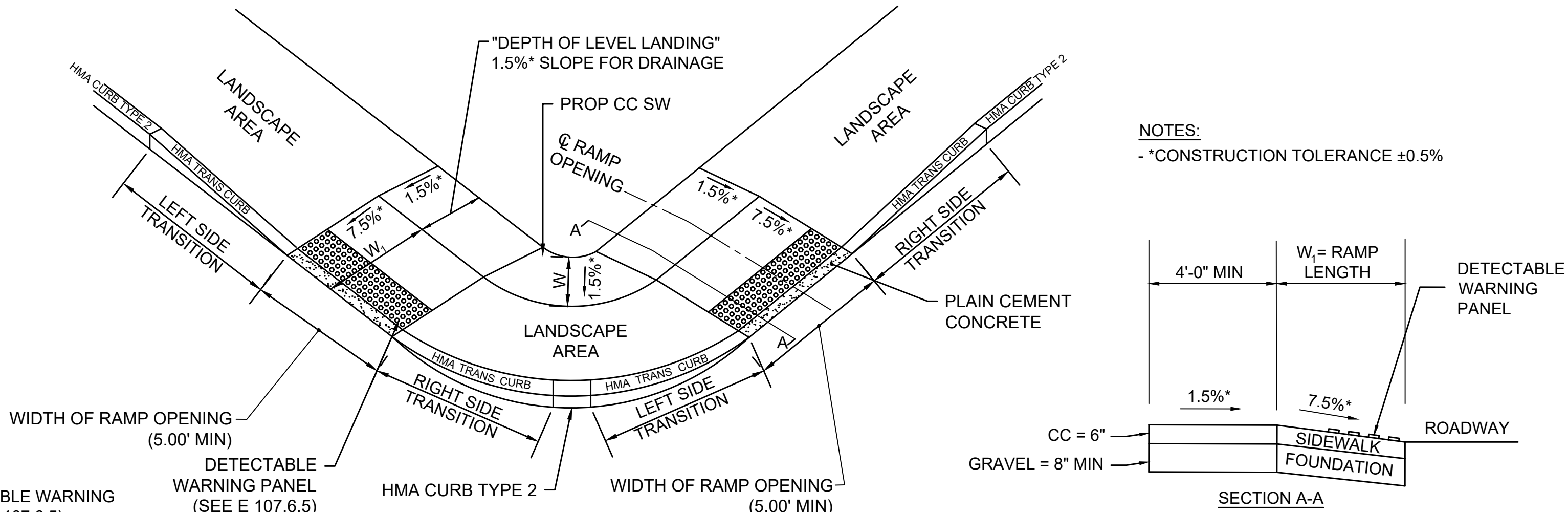
**PEDESTRIAN CURB RAMP – NARROW SIDEWALK**  
**REFER TO MASSDOT STD. DTL. E 107.2.1**  
N.T.S.



PCR #	ALIGNMENT	RAMP REFERENCE POINT		WIDTH OF SIDEWALK (W)	WIDTH OF RAMP OPENING MIN 5.00'	ROADWAY GUTTER SLOPE	TRANSITION LENGTH	
		STATION	OFFSET				LEFT	RIGHT
3	LINCOLN ST	102+02.92	12.43' LT	5.00'	5.00'	-5.99%	6'-0"	15'-0"
4**	LINCOLN ST	104+87.78	14.47' LT	5.00'	5.00'	-2.37%	6'-6"	6'-0"

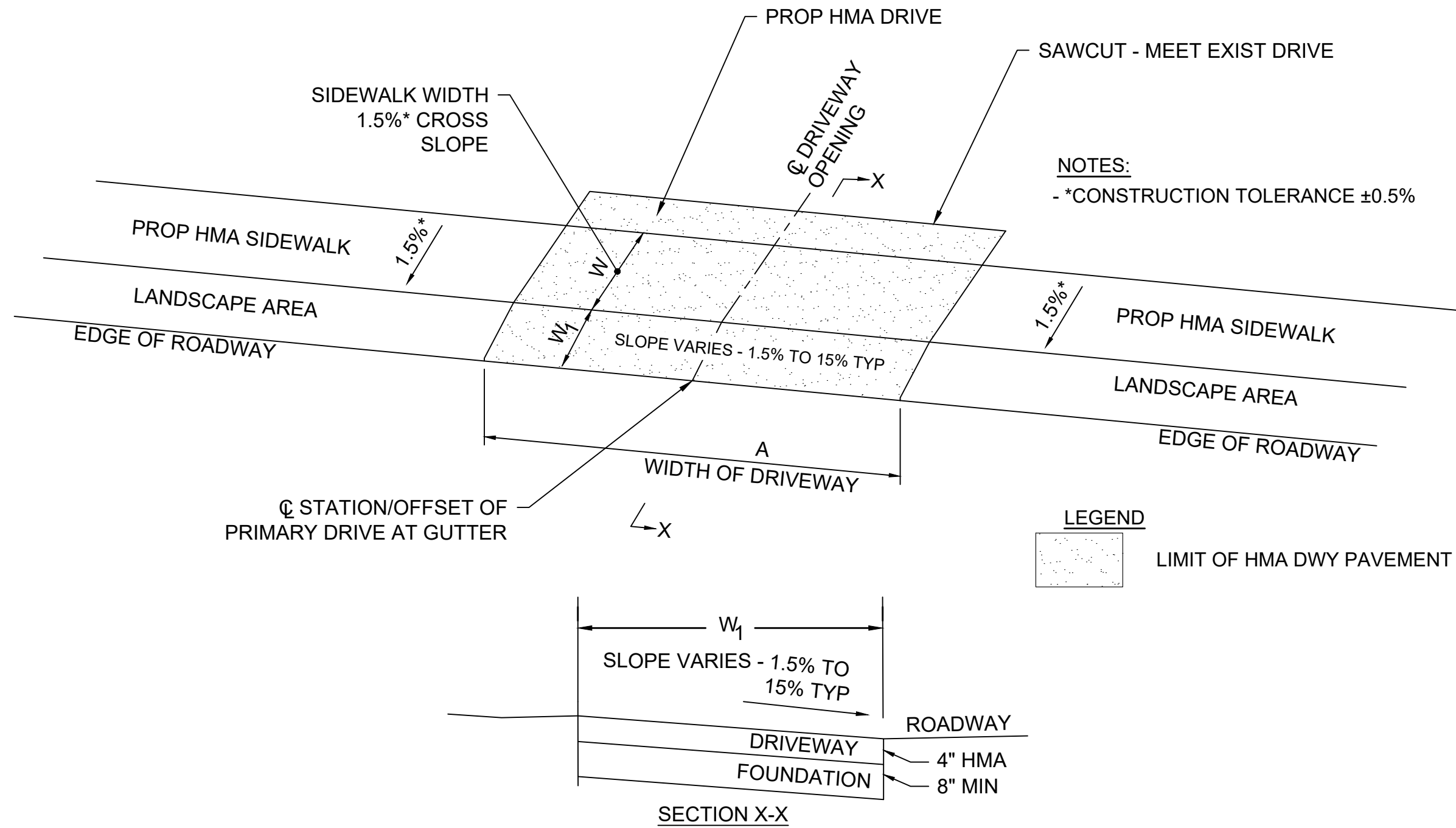
\*\* THE LANDSCAPED AREA IS ON THE RIGHT SIDE AND THE CC TRANSITION IS ON THE LEFT SIDE OF THIS PCR

**PEDESTRIAN CURB RAMP – ONE-SIDED NARROW SIDEWALK**  
**REFER TO MASSDOT STD. DTL. E 107.2.1**  
N.T.S.



PCR #	ALIGNMENT	RAMP REFERENCE POINT		LENGTH OF PRIMARY RAMP (W1)	WIDTH OF SIDEWALK (W)	WIDTH OF RAMP OPENING MIN 5.00'	DEPTH OF LEVEL LANDING MIN 4.0'	TRANSITION LENGTH	
		STATION	OFFSET					LEFT	RIGHT
1	WESTON ST	11+42.62	14.58' RT	3.00'	5.00'	5.00'	5.00'	6'-0"	6'-0"
2	LINCOLN ST	102+04.04	14.84' RT	3.00'	5.00'	5.00'	5.00'	6'-0"	6'-0"
5	LINCOLN ST	104+87.78	13.22' RT	5.00'	5.00'	5.00'	5.00'	6'-0"	6'-0"
6	MIDDLE ST	20+27.62	12.31' RT	5.00'	5.00'	5.00'	5.00'	6'-0"	6'-0"
11	LINCOLN ST	111+63.65	17.89' LT	5.00'	5.00'	5.00'	5.00'	6'-0"	6'-0"
12	BIRCH HILL LN	30+30.43	14.87' LT	5.00'	5.00'	5.00'	5.00'	6'-0"	6'-0"

**PEDESTRIAN CURB RAMP ON CORNER WITH LANDSCAPING STRIP**  
N.T.S.



DWY #	ALIGNMENT	RAMP REFERENCE POINT		WIDTH OF SIDEWALK (W)	LENGTH OF PRIMARY RAMP (W1)	WIDTH OF DRIVEWAY (A)
		STATION	OFFSET			
5	LINCOLN ST	106+66.05	9.25' RT	5.0'	6.50'	20.25'
6*	LINCOLN ST	107+18.10	10.96' RT	5.0'	6.60'	18.61'
8	LINCOLN ST	118+80.81	13.60' RT	5.0'	12.13'	16.00'
9	LINCOLN ST	121+80.06	11.36' RT	5.0'	3.00'	20.14'
10**	LINCOLN ST	122+31.65	13.10' RT	5.0'	3.00'	16.39'
11**	LINCOLN ST	124+09.73	13.21' RT	5.0'	3.00'	15.74'
12**	LINCOLN ST	124+51.97	13.07' RT	5.0'	3.00'	22.90'
13**	LINCOLN ST	126+08.43	10.30' RT	5.0'	3.00'	16.00'

\* CONSTRUCTION OF THE PRIMARY DRIVEWAY RAMP IS NOT PROPOSED. EXISTING COBBLESTONES SHALL BE RETAINED.  
\*\* THE CROSS-SLOPE OF PRIMARY RAMP (W1) OF DRIVEWAYS 10, 11, 12, AND 13 SHALL MATCH THE EXISTING CONDITIONS CROSS-SLOPE BUT SHALL NOT EXCEED 15%. THE MAXIMUM CROSS-SLOPE OF THE SIDEWALK SHALL BE 1.5% ±0.5%.

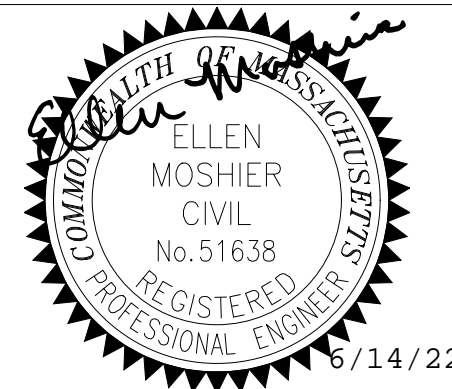
**HMA SIDEWALK THROUGH DRIVEWAY WITH LANDSCAPING STRIP**  
NOT TO SCALE



**TOWN OF LEXINGTON, MASSACHUSETTS**

**HIGHWAY DEPARTMENT**

**LINCOLN STREET SIDEWALK PROJECT**



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**CONSTRUCTION DETAILS - 5**

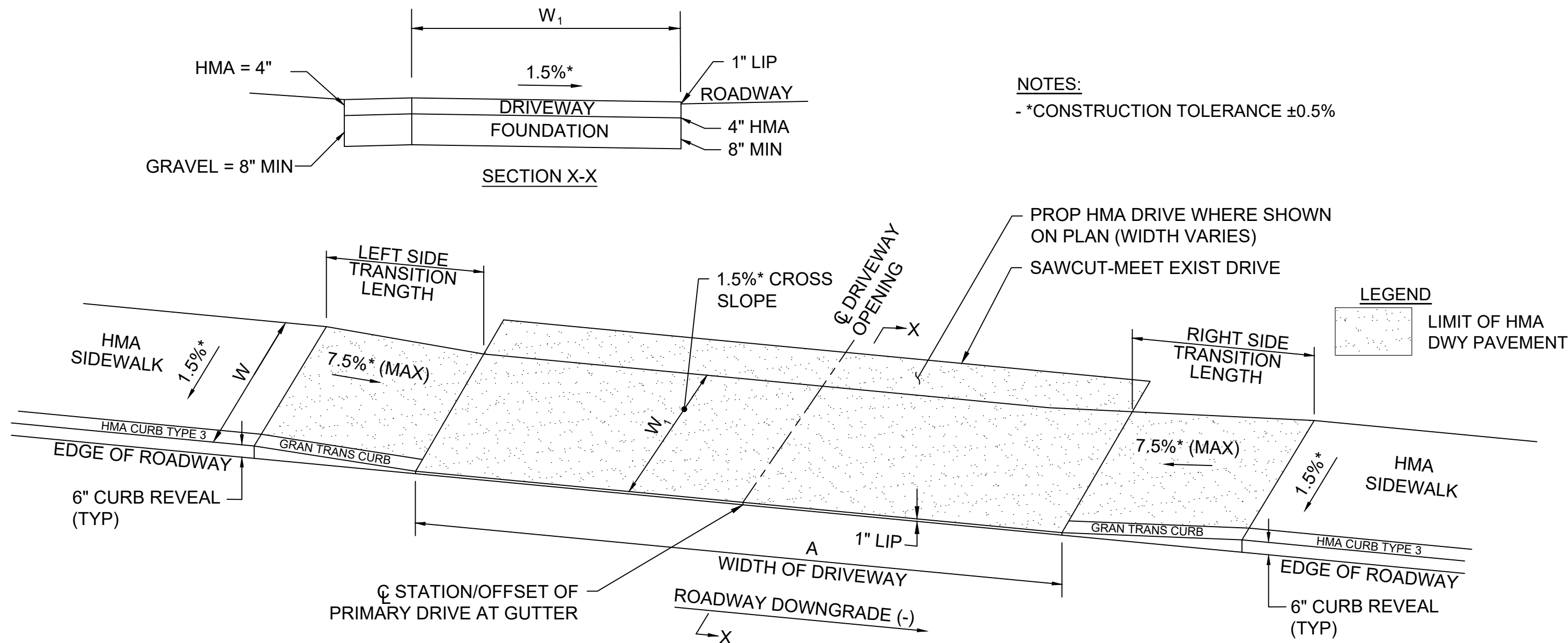
No.	Submittal / Revision	App'd. By	Date
1	CONCEPT		10/22/21
2	CONCEPT - REV 1		01/05/22
3	PRELIM		03/02/22
4	PRELIM - REV 1		03/09/22
5	FINAL		06/02/22
6	FINAL REV 1		06/14/22

Designed By: DC/JB	Drawn By: DC/JB	Checked By: JGM
Issue Date: 06-14-2022	Project No: 71414	Scale: NTS

Drawing No.:

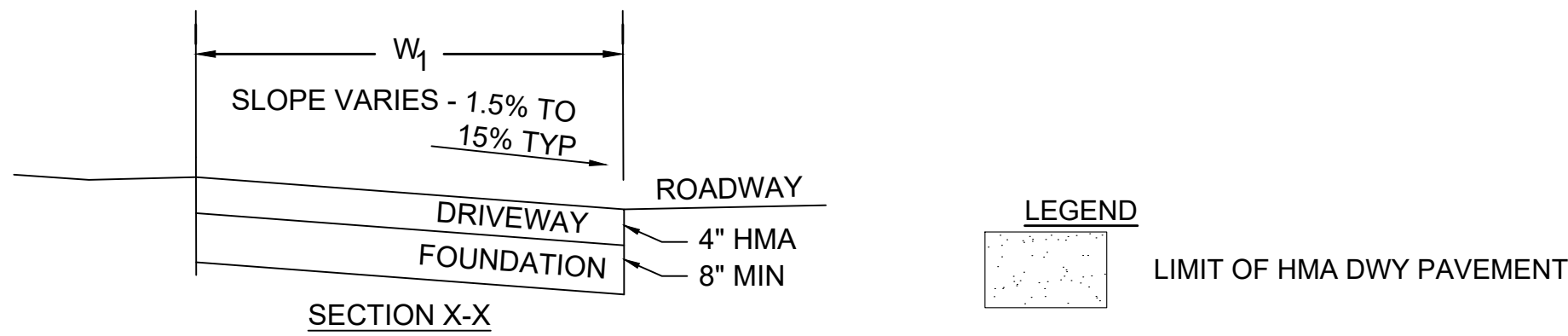
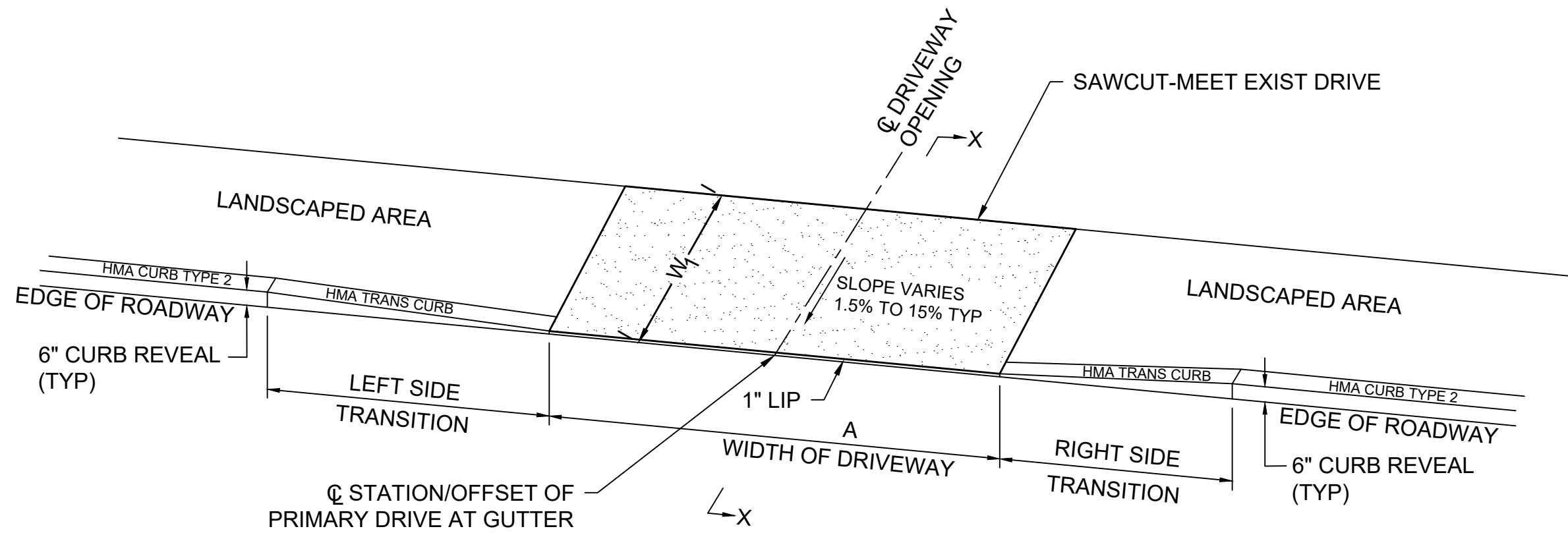


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Saved: 6/10/2022 4:46:19 PM Plotted: 6/13/2022 4:53:52 PM Current User: Chander, Daren Last Saved By: 6193



DWY #	ALIGNMENT	DWY REFERENCE POINT C		WIDTH OF SIDEWALK (W)	PATH OF TRAVEL (MIN 3.00') (W <sub>1</sub> )	WIDTH OF DRIVEWAY (A)	ROADWAY GUTTER**	TRANSITION LENGTH	
		STATION	OFFSET					LEFT SIDE	RIGHT SIDE
1	LINCOLN ST	102+58.84	12.61' LT	5.5'	5.5'	16.02'	5.91%	6'-6"	15'-0"
2	LINCOLN ST	103+88.50	13.08' LT	5.5'	5.5'	16.56'	6.42%	6'-6"	15'-0"
7	LINCOLN ST	113+62.01	9.34' RT	5.5'	5.5'	14.05'	0.56%	6'-6"	7'-8"
14	LINCOLN ST	126+81.61	9.82' RT	5.5'	5.5'	24.89'	0.70%	6'-6"	7'-8"

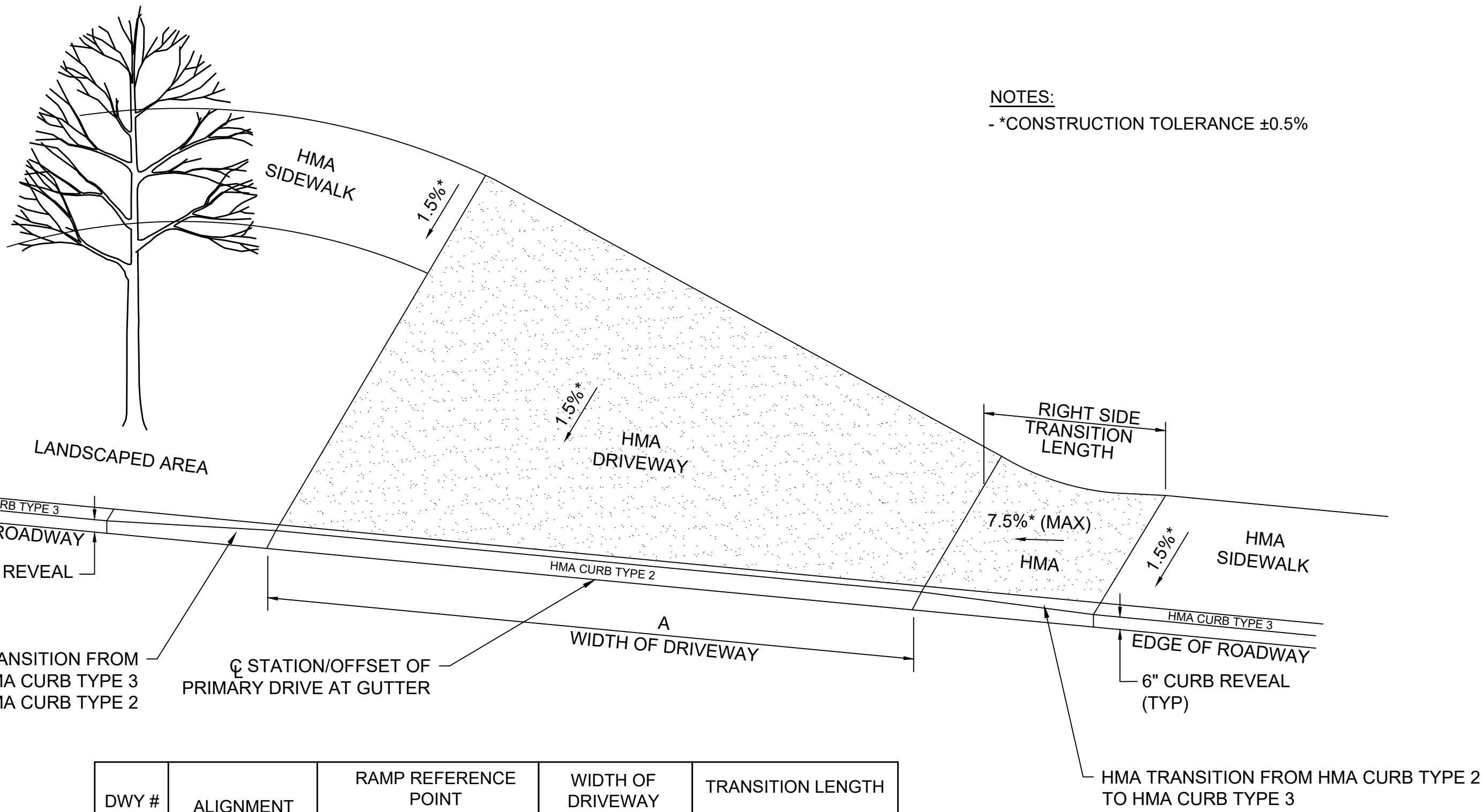
**HMA SIDEWALK THROUGH DRIVEWAYS**  
**REFER TO MASSDOT STD. DTL. E 107.7.0**  
NOT TO SCALE



DWY #	ALIGNMENT	RAMP REFERENCE POINT		LENGTH OF DRIVEWAY (W <sub>1</sub> )	WIDTH OF DRIVEWAY (A)	TRANSITION LENGTH	
		STATION	OFFSET			LEFT	RIGHT
3	LINCOLN ST	104+43.34	13.00' RT	VARIES (8.1-13.6')	21.49'	6'-0"	6'-0"
4	MIDDLE ST	20+40.40	12.00' RT	VARIES (14.1-30.3')	19.65'	6'-0"	-*
15	WESTON ST	11+26.42	15.38' RT	4.54'	13.32'	6'-0"	-

\* NO CURB ON RIGHT SIDE OF DWY 4 DUE TO PROXIMITY TO PEDESTRIAN CURB RAMP

**DRIVEWAY THROUGH LANDSCAPE AREA**  
NOT TO SCALE



DWY #	ALIGNMENT	RAMP REFERENCE POINT		WIDTH OF DRIVEWAY (A)	TRANSITION LENGTH	
		STATION	OFFSET		LEFT	RIGHT
16	LINCOLN ST	114+12.90	8.57' RT	13.41'	6'-0"	6'-0"

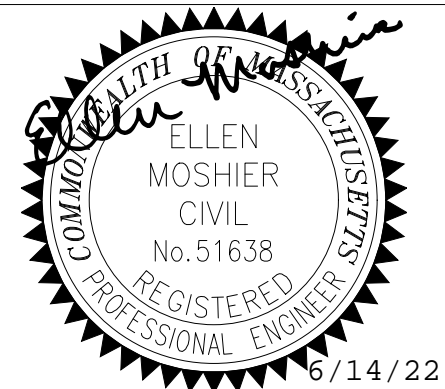
**DRIVEWAY #16 DETAIL**  
NOT TO SCALE



TOWN OF  
LEXINGTON,  
MASSACHUSETTS

HIGHWAY  
DEPARTMENT

LINCOLN STREET  
SIDEWALK  
PROJECT



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**CONSTRUCTION  
DETAILS - 6**

No.	Submittal / Revision	App'd. By	Date
1	CONCEPT		10/22/21
2	CONCEPT - REV 1		01/05/22
3	PRELIM		03/02/22
4	PRELIM - REV 1		03/09/22
5	FINAL		06/02/22
6	FINAL REV 1		06/14/22

Designed By: DC/JB	Drawn By: DC/JB	Checked By: JGM
Issue Date: 06-14-2022	Project No: 71414	Scale: NTS

Drawing No.:

18 OF 20



IDENTIFI- CATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	BACK- GROUND	COLOR	BORDER	NUMBER OF POSTS REQUIRED	UNIT AREA (S.F.)	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.							
MA-R2-10a	48"	36"	WORK ZONES SPEEDING FINES DOUBLED	SEE MASSDOT STANDARD SIGN BOOK			6	FLUOR- ESCENT ORANGE & WHITE	BLACK	BLACK	6	12.00	72.00
MA-R2-10e	36"	48"	END ROAD WORK DOUBLE FINES END	SEE MASSDOT STANDARD SIGN BOOK			6	FLUOR- ESCENT ORANGE & WHITE	BLACK	BLACK	6	12.00	72.00
MA-W20-7b	36"	36"	POLICE OFFICER AHEAD	SEE MASSDOT STANDARD SIGN BOOK			4	FLUOR- ESCENT ORANGE	BLACK	BLACK	4	9.00	36.00
W1-4R	36"	36"		①	①	①	2	FLUOR- ESCENT ORANGE	BLACK	BLACK	2	9.00	18.00
W1-4L	36"	36"					2	FLUOR- ESCENT ORANGE	BLACK	BLACK	2	9.00	18.00
W5-1	36"	36"	ROAD NARROWS				2	FLUOR- ESCENT ORANGE	BLACK	BLACK	2	9.00	18.00
W13-1	24"	24"	XX M.P.H.				2	FLUOR- ESCENT ORANGE	BLACK	BLACK	0 (MOUNTED W/ WARNING SIGN)	4.00	8.00
W20-1	36"	36"	ROAD WORK AHEAD				10	FLUOR- ESCENT ORANGE	BLACK	BLACK	10	9.00	90.00
W20-4	36"	36"	ONE LANE ROAD AHEAD				4	FLUOR- ESCENT ORANGE	BLACK	BLACK	4	9.00	36.00

① SEE MUTCD 2009 EDITION AND 2012 SUPPLEMENT  
TO THE 2004 STANDARD HIGHWAY SIGNS

LEGEND

- ◉

REFLECTORIZED DRUM
- ➔

PROPOSED TRAFFIC FLOW
- WORK ZONE
- ⌚

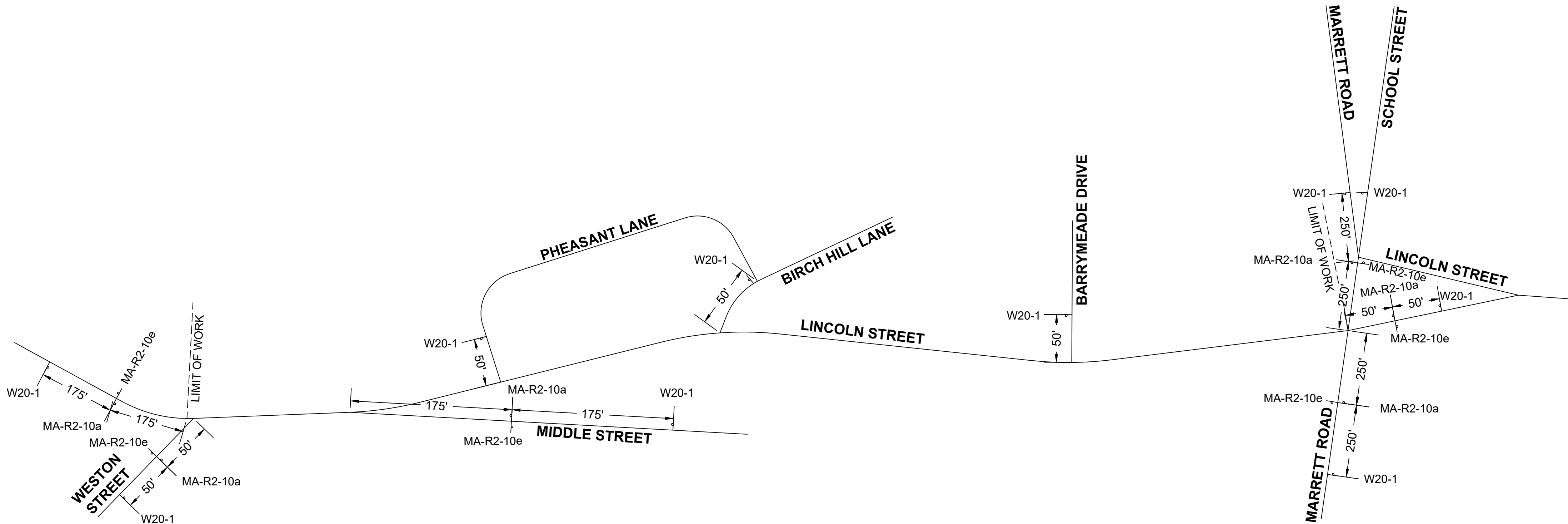
TYPE III BARRICADE
- Ⓟ

POLICE DETAIL
- ⚓

CONSTRUCTION SIGN
- N.T.S.

NOT TO SCALE
- Ⓢ

FLAGGER



ADVANCED WARNING SIGN SCHEMATIC  
N.T.S.

GENERAL NOTES

- ALL CONSTRUCTION SIGNING, DRUMS, BARRICADES AND OTHER DEVICES SHALL CONFORM WITH PART 6 OF THE LATEST EDITION MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.)
- ALL CHANNELIZING DEVICES SHALL BE SET @ 25' O.C. MAX. UNLESS OTHERWISE NOTED OR ADJUSTED BY THE RESIDENT ENGINEER.
- ALL DRUMS SHALL BE APPROPRIATELY PLACED AND MOVED AS NECESSARY TO MAINTAIN ADEQUATE ABUTTER ACCESS AT ALL TIMES. WORK MAY REQUIRE ADDITIONAL SIGNS, DRUMS AND OTHER TRAFFIC CONTROL DEVICES, GRADING AND TEMPORARY PAVEMENT FOR PASSAGE OF PEDESTRIAN, VEHICULAR AND EMERGENCY TRAFFIC THROUGH THE WORK AREAS, BOTH DURING AND AFTER WORK HOURS, TO MAINTAIN SUCH ACCESS.
- THE CONTRACTOR SHALL NOTIFY EACH ABUTTER AT LEAST 24 HOURS IN ADVANCE OF THE START OF ANY WORK THAT WILL REQUIRE THE TEMPORARY CLOSURE OF ACCESS, SUCH AS EXISTING PAVEMENT EXCAVATION, TEMPORARY DRIVEWAY PAVEMENT PLACEMENT AND SIMILAR OPERATIONS.
- THE CONTRACTOR SHALL NOTIFY THE TOWN OF LEXINGTON POLICE, FIRE, AND HIGHWAY DEPARTMENT AT LEAST 48 HOURS IN ADVANCE OF LANE CLOSURES.
- A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT DURING WORKING HOURS, TRAFFIC MAY BE REDUCED TO ONE LANE UNDER POLICE OR FLAGGER CONTROL FOR SHORT TIME PERIODS WHEN REQUIRED FOR THE WORK, AS SHOWN.
- GRADE SEPARATIONS IN EXCESS OF 2 INCHES DURING NON-WORKING HOURS WILL REQUIRE DELINEATION BY USE OF DRUMS.
- EXCAVATION EDGES IN EXCESS OF 4 INCHES DEEP SHALL BE PROTECTED DURING NON-WORKING HOURS BY BACKFILLING WITH A WEDGE OF GRAVEL OR SOIL COMPACTED TO A 4:1 SLOPE.
- 11' MINIMUM LANE WIDTHS SHALL BE MAINTAINED DURING CONSTRUCTION. IF THIS IS NOT POSSIBLE, LANE WIDTHS SHALL BE EQUAL TO OR WIDER THAN THE EXISTING CONDITION LANE WIDTHS.
- NON-ESSENTIAL TEMPORARY CONSTRUCTION TRAFFIC CONTROL DEVICES SHALL BE COVERED OR REMOVED DURING NON-WORKING HOURS.
- ADVISORY SPEED PLATES (W13-1) SHALL BE USED IF APPROPRIATE AND AS DIRECTED BY THE ENGINEER.
- ALL TEMPORARY PEDESTRIAN PATHWAYS SHALL COMPLY FULLY WITH ALL REQUIREMENTS OF THE MUTCD, ALL APPLICABLE MASS ARCHITECTURAL ACCESS BOARD (MAAB) REQUIREMENTS, ALL AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG) REQUIREMENTS, AND THE MASSDOT STANDARD DETAILS AND DRAWINGS FOR THE DEVELOPMENT OF TEMPORARY TRAFFIC CONTROL PLANS, SPECIFICALLY THE PEDESTRIAN DETAILS PED-1 THROUGH PED-8.
- CONTRACTOR SHALL MAINTAIN ADA-COMPLIANT PEDESTRIAN ACCESS AT ALL TIMES, SPECIFICALLY INCLUDING PEDESTRIAN GUIDANCE SYSTEMS AT WORK ZONES. ACCESS SHALL BE MAINTAINED ALONG ALL SIDEWALKS AND CROSSWALKS AND TO ALL ABUTTERS. ANY PEDESTRIAN DETOURS SHALL INCLUDE A FULLY ADA COMPLIANT PEDESTRIAN DETOUR ROUTE WITH PROPER BARRICADES, RAILINGS, RAMPS, AND SIGNAGE.



TOWN OF  
LEXINGTON,  
MASSACHUSETTS

HIGHWAY  
DEPARTMENT

LINCOLN STREET  
SIDEWALK  
PROJECT



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TEMPORARY  
TRAFFIC  
CONTROL  
PLANS - 1

No.	Submittal/ Revision	App'd. By	Date
1	CONCEPT		10/22/21
2	CONCEPT - REV 1		01/05/22
3	PRELIM		03/02/22
4	PRELIM - REV 1		03/09/22
5	FINAL		06/02/22
6	FINAL REV 1		06/14/22

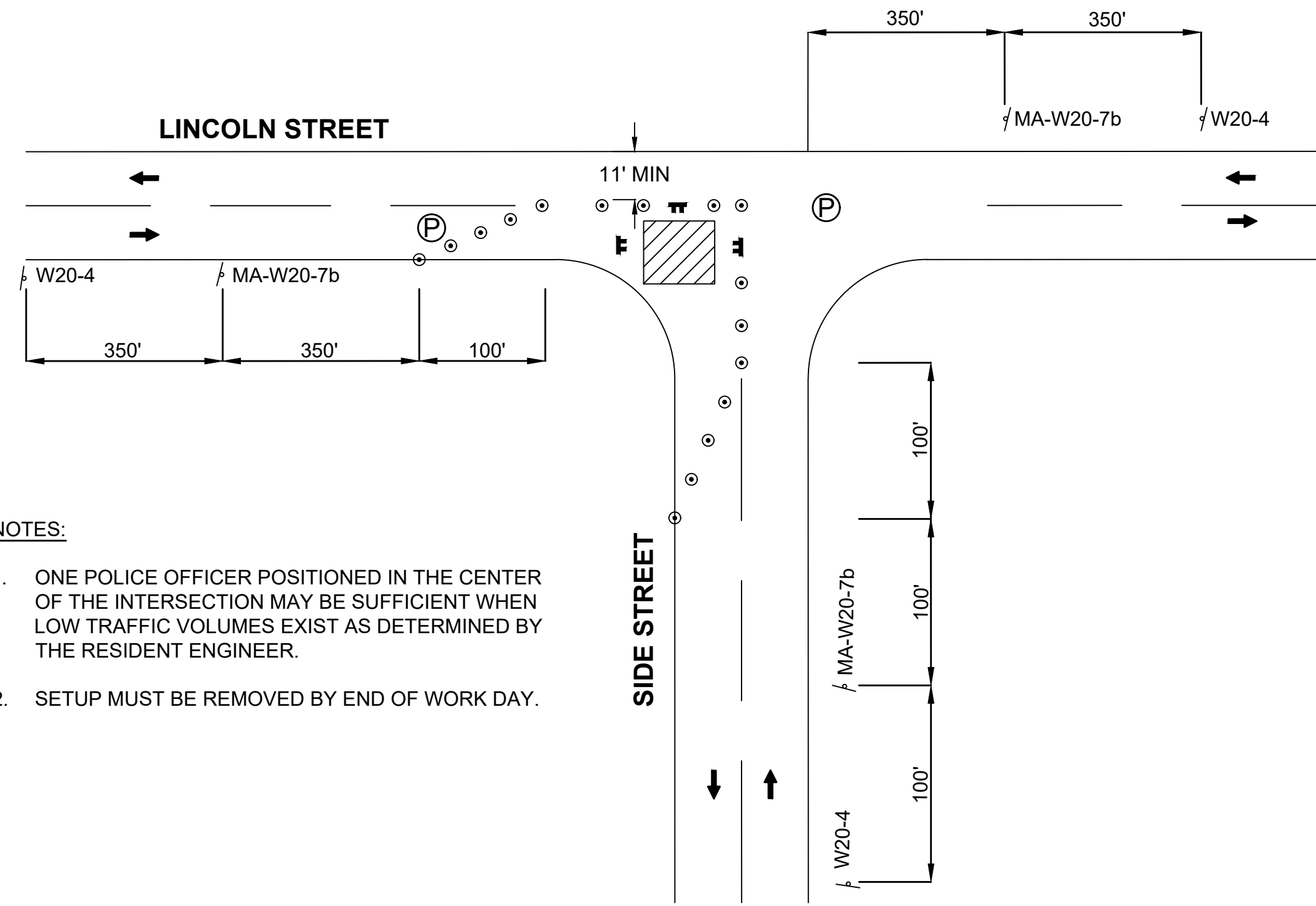
Designed By: DC/JB	Drawn By: DC/JB	Checked By: JGM
Issue Date: 06-14-2022	Project No: 71414	Scale: NTS

Drawing No.:

19 OF 20



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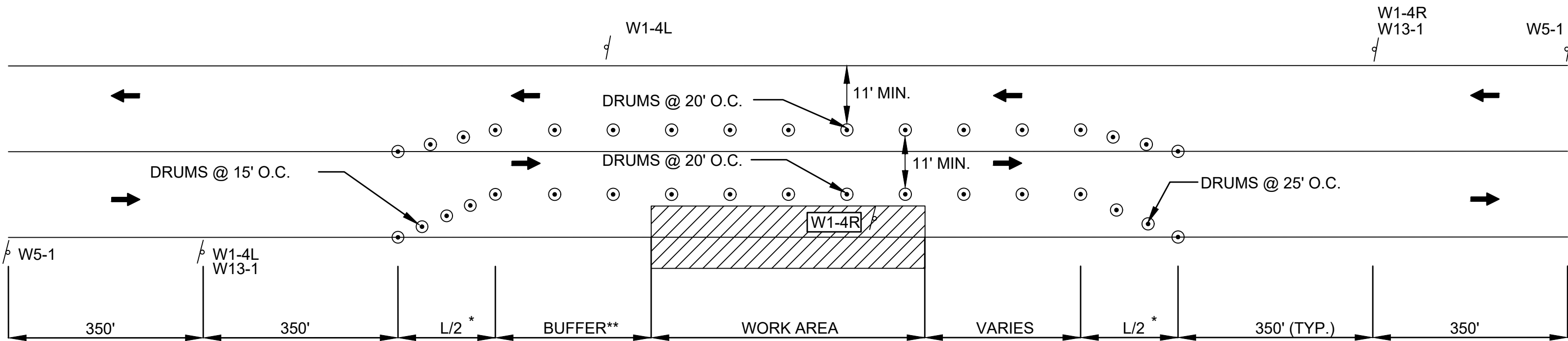


NOTES:

1. ONE POLICE OFFICER POSITIONED IN THE CENTER OF THE INTERSECTION MAY BE SUFFICIENT WHEN LOW TRAFFIC VOLUMES EXIST AS DETERMINED BY THE RESIDENT ENGINEER.
2. SETUP MUST BE REMOVED BY END OF WORK DAY.

TYPICAL MOVABLE WORK AREA "OPERATIONAL"  
(DAY-TO-DAY) PLAN

N.T.S.

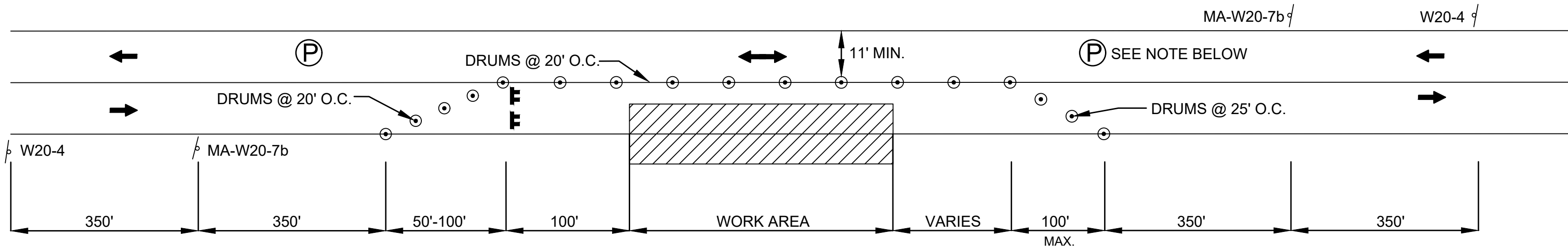


\*  $L = \frac{WS^2}{60}$   
L=TAPER LENGTH  
W=WIDTH OF ROADWAY TO BE SHIFTED OR REDIRECTED  
S=POSTED SPEED LIMIT

TYPICAL TWO WAY STREET LANE SHIFT

N.T.S.

\*\* SEE TABLE 1 FOR APPROPRIATE BUFFER BASED UPON SPEED



TYPICAL TWO WAY STREET LANE CLOSURE

N.T.S.

NOTE:  
WORK THAT WILL DISRUPT EXISTING TRAFFIC PATTERNS WITHIN THE EXISTING ROADWAYS (LANE REDUCTION, ETC.) SHALL ONLY BE DONE DURING OFF PEAK HOURS. NUMBERS AND LOCATION OF POLICE DETAILS FOR WORK ZONE AND/OR DETOUR MANAGEMENT SHALL BE DETERMINED BY THE RESIDENT ENGINEER ON A CASE BY CASE BASIS, AS WORK PROGRESSES.

LEGEND

- ⊙ CHANNELIZING DEVICE
- PROPOSED TRAFFIC FLOW
- ▨ WORK ZONE
- ⌵ TYPE III BARICADE
- Ⓟ POLICE DETAIL
- ⏏ CONSTRUCTION SIGN
- N.T.S. NOT TO SCALE

TABLE 1 - BUFFERS

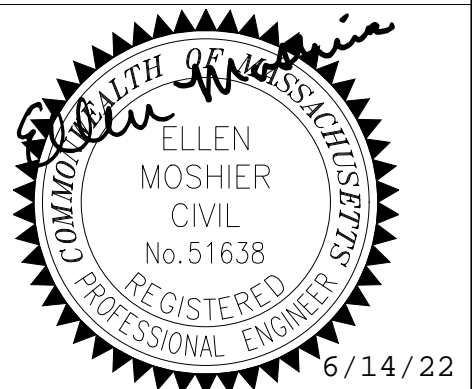
SPEED (MPH)	DISTANCE (FT)
20	115
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645
70	730
75	820



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TEMPORARY  
TRAFFIC  
CONTROL  
PLANS - 2

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1	CONCEPT		10/22/21
2	CONCEPT - REV 1		01/05/22
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